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Res 720, 97-69
Date 10/6/97
Linda [unclear]

ORDINANCE NO. 3372

AN ORDINANCE OF THE CITY OF HUNTINGTON BEACH AMENDING THE
HUNTINGTON BEACH ZONING AND SUBDIVISION ORDINANCE BY ADOPTING THE
MCDONNELL CENTRE BUSINESS PARK SPECIFIC PLAN NO. 11

AND

CHANGING THE ZONING DESIGNATION FROM LIMITED INDUSTRIAL (IL) AND
LIMITED INDUSTRIAL-HIGH RISE OVERLAY (IL-H) TO MCDONNELL CENTRE
BUSINESS PARK SPECIFIC PLAN NO. 11 FOR THE AREA GENERALLY BOUNDED BY
RANCHO ROAD, SPRINGDALE STREET, BOLSA AVENUE, AND
BOLSA CHICA STREET IN THE CITY OF HUNTINGTON BEACH
(ZMA NO. 96-6; ZTA NO. 96-1)

WHEREAS, pursuant to the State Planning and Zoning Law, the Huntington Beach Planning Commission and Huntington Beach City Council have held separate public hearings relative to Zoning Map Amendment No. 96-6 and Zoning Text Amendment No. 96-1, wherein both bodies have carefully considered all information presented at said hearings, and after due consideration of the findings and recommendations of the Planning Commission and all evidence presented to said City Council, the City Council finds that such zoning map and text amendments are proper, and consistent with the General Plan.

NOW, THEREFORE, the City Council of the City of Huntington Beach does ordain as follows:

SECTION 1. That the real property subject to this ordinance, hereinafter referred to as the "Subject Property," is generally described as the approximately 307 acre site bounded by Rancho Road on the north, Springdale Street on the east, Bolsa Avenue on the south, and Bolsa Chica Road on the west, and is more particularly described in the legal description and sketch attached hereto as Exhibits "A-1" and "A-2," respectively, and incorporated by this reference as though fully set forth herein.

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NOV 20 1997

SECTION 2. That the McDonnell Centre Business Park Specific Plan No. 11, attached hereto as Exhibit "B" and incorporated by this reference as though fully set forth herein, is hereby approved and adopted.

SECTION 3. That the zoning designation of the Subject Property is hereby changed from Limited Industrial (IL) and Limited Industrial-High Rise Overlay (IL-H) to McDonnell Centre Business Park Specific Plan No. 11.

SECTION 4. That the Director of Community Development is hereby directed to amend Sectional District Map No. 9 of the Huntington Beach Zoning and Subdivision Code to 9-5-11 reflect the changes contained in this ordinance. The Director of Community Development is further directed to file the amended map. A copy of such map, as amended, shall be available for inspection in the Office of the City Clerk.

SECTION 5. This ordinance shall take effect thirty days after its adoption.

PASSED AND ADOPTED by the City Council of the City of Huntington Beach at a regular meeting thereof held on the 20th day of October, 1997.

Roger H. Bauer
Mayor

ATTEST:

Lonnie Brochway
City Clerk

APPROVED AS TO FORM:

Scott Field
City Attorney Jul 1/30/97

~~REVIEWED AND APPROVED:~~

Matthew E. Uberuogara
City Administrator

INITIATED AND APPROVED:

Melanie X. Freeman
Director of Community Development

Exhibits: A-1: Legal Description
A-2: Sketch
B: McDonnell Centre Business Park Specific Plan No. 11

A. LEGAL DESCRIPTION

The following provides the legal description for the entire McDonnell Centre Business Park Specific Plan area, which is bounded by an at grade spurtrack of the US Navy (railroad right-of-way) and Rancho Road on the north, Springdale Street on the east, Bolsa Avenue on the south and Bolsa Chica Street and the Orange County Flood Control District channel on the west. Refer to Exhibit 1 in the Specific Plan.

A portion of fractional Section 9, Township 5 South, Range 11 West, partly in the Rancho Las Bolsas and partly in the Rancho La Bolsa Chica, in the City of Huntington Beach, County of Orange, State of California, as per map recorded in Book 51, page 13 of Miscellaneous Maps, in the Office of the County Recorder of said county, described as follows:

Commencing at the Southeast Corner of said section, said point being also the center line intersection of Springdale Street and Bolsa Avenue as shown on a map of Parcel Map No. 93-202, recorded in Book 281, pages 3 and 4 of Parcel Maps, in the Office of said County Recorder; thence North $0^{\circ}16'04''$ East 2055.80 feet along the center line of Springdale Street, said center line being also the easterly line of said section; thence North $89^{\circ}28'58''$ West 50.00 feet to a point on the westerly line of Springdale Street as shown on said parcel map, said point being also the northeast corner of Parcel 3 of said parcel map and the Point of Beginning; thence, along said westerly line and parallel with the easterly line of said section, South $0^{\circ}16'04''$ West 1965.58 feet to the southerly terminus of the easterly line of Parcel 1 of Parcel Map No. 95-136 as shown on a map thereof recorded in Book 288, pages 49 and 50 of Parcel Maps; thence, along a tangent 30.00 foot radius curve, concave northwesterly, through a central angle of $90^{\circ}18'36''$, a distance 47.29 feet to a tangent line which is parallel with and 60.00 feet

northerly of the southerly line of said section, said last mentioned southerly line being also the center line of Bolsa Avenue as shown said last mentioned parcel map, said tangent line being also the southerly line of said Parcel 1; thence along said tangent line, North $89^{\circ}25'20''$ West 23.70 feet to tangent 38.00 foot radius curve, concave northerly; thence, continuing along the southerly line of said Parcel 1 and said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 5.80 feet; thence, tangent to said curve, North $80^{\circ}40'34''$ West 58.12 feet to the beginning of a tangent 62.00 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 9.46 feet; thence, tangent to said curve, North $89^{\circ}25'20''$ West 52.35 feet to the beginning of a tangent 62.00 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 9.46 feet; thence tangent to said curve, South $81^{\circ}49'54''$ West 58.12 feet to a tangent 38.00 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 5.80 feet to a tangent line which is parallel with and 60.00 feet northerly of the southerly line of said section; thence, along said last mentioned tangent line, North $89^{\circ}25'20''$ West 4117.58 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23 feet; thence, tangent to said curve, North $72^{\circ}43'24''$ West 20.88 feet to the beginning of a tangent 83.13 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence, tangent to said curve, North $89^{\circ}25'20''$ West 49.12 feet to the beginning of a tangent 83.13 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence, tangent to said curve, South $73^{\circ}52'45''$ West 20.88 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23

feet to a tangent line which is parallel with and 60.00 feet northerly of the southerly line of said section; thence, along said last mentioned tangent line, North $89^{\circ}25'20''$ West 597.93 feet to the westerly terminus of the southerly line of Parcel 2 of Parcel Map No. 87-424, as shown on a map thereof recorded in Book 237, pages 4 through 6, inclusive, of Parcel Maps in the Office of said County Recorder; thence, along a tangent 30.00 foot radius curve, concave northeasterly, through a central angle of $90^{\circ}02'30''$, a distance of 47.15 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map, said tangent line being also the westerly line of said Parcel 2; thence, along said tangent line, North $0^{\circ}37'10''$ East 221.35 feet to a tangent 180.75 foot radius curve, concave easterly; thence, continuing along the westerly line of said parcel and along said tangent curve, through a central angle of $9^{\circ}26'16''$, a distance of 29.77 feet; thence, tangent to said curve, North $10^{\circ}03'26''$ East 31.14 feet to a tangent 180.75 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of $9^{\circ}26'16''$, a distance of 29.77 feet; thence, tangent to said curve, North $10^{\circ}03'26''$ East 227.87 feet to the beginning of a non-tangent 18.00 foot radius curve, concave northeasterly, a radial to the beginning of said curve bears South $27^{\circ}00'26''$ West; thence, along said non-tangent curve, through a central angle of $63^{\circ}36'44''$, a distance of 19.98 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map; thence, along said tangent line, North $0^{\circ}37'10''$ East 49.56 feet to a tangent 59.13 foot radius curve, concave easterly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23 feet; thence, tangent to said curve, North $17^{\circ}19'06''$ East 20.88 feet to the beginning of a tangent 83.13 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence, tangent to said curve, North $0^{\circ}37'10''$ East 49.12 feet to the beginning of a tan-

gent 83.13 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence tangent to said curve, North $16^{\circ}04'45''$ West 20.88 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map; thence, along said tangent line, North $0^{\circ}37'10''$ East 2,031.46 feet to a tangent 30.00 foot radius curve, concave southeasterly; thence, along said tangent curve, through a central angle of $49^{\circ}52'31''$, 26.11 feet to a tangent line which is parallel with and 50.00 feet southeasterly of the center line of Rancho Avenue as shown on a Record of Survey recorded in Book 60, page 13 of Record of Surveys; thence, along said tangent line, North $50^{\circ}29'41''$ East 2382.14 feet to the southerly line of the 75.00 foot wide Right-of-Way described in deed recorded in Book 1296, page 88 of Deeds in the Office of the County Recorder of said county; thence, along said southerly line, South $39^{\circ}28'30''$ East 877.72 feet to a tangent 1,947.78 foot radius curve, concave northeasterly; thence, along said tangent curve and said southerly line, through a central angle of $50^{\circ}00'28''$, 1,700.02 feet to a tangent line, said tangent line being also the westerly prolongation of the northerly line of Parcel 1 of said previously mentioned Parcel Map No. 93-202; thence, along said tangent line, South $89^{\circ}28'58''$ East 492.00 feet to the northeasterly corner of said last mentioned Parcel 1 and the beginning of a non-tangent 383.39 foot radius curve, concave southeasterly, a radial to the beginning of said curve bears North $32^{\circ}30'55''$ East; thence, along the northeasterly line of said parcel and said non-tangent curve, through a central angle of $47^{\circ}51'40''$, 320.26 feet to the northwesterly corner of Parcel 2 of said Parcel Map No. 93-202; thence, along the northerly line of Parcel 2 and Parcel 3 of said parcel map, South $89^{\circ}28'58''$ East 598.07 feet to the Point of Beginning.
Containing 308.44 acres, more or less.

CITY OF
HUNTINGTON BEACH
ORANGE COUNTY, CALIFORNIA

ADOPTED JUNE 28, 1960

CITY COUNCIL ORDINANCE NO. 779

<u>AMENDED</u>	<u>ZONE</u> <u>CASE</u>	<u>ORD NO.</u>	<u>AMENDED</u>	<u>ZONE</u> <u>CASE</u>	<u>ORD NO.</u>
11-3-80	80-14	2459			
1-17-83	FLOOD ZONE	2606			

NOTE:
ALL DIMENSIONS ARE IN FEET
ANY ZONE ADJOINING ANY RIGHT OF WAY
IS INTENDED TO EXTEND TO THE CENTER
OF SUCH RIGHT OF WAY

LEGEND:

(R1)	LOW DENSITY RESIDENTIAL DISTRICT
(M1A)	RESTRICTED MANUFACTURING DISTRICT
(C1F)	COMMUNITY FACILITIES (EDUCATION) DISTRICT
(MS)	MULTISTORY DISTRICT
(FP2)	FLOODPLAIN DISTRICT
----	PRIVILEGE STREET
///	WITHIN FLOOD ZONE +FP2

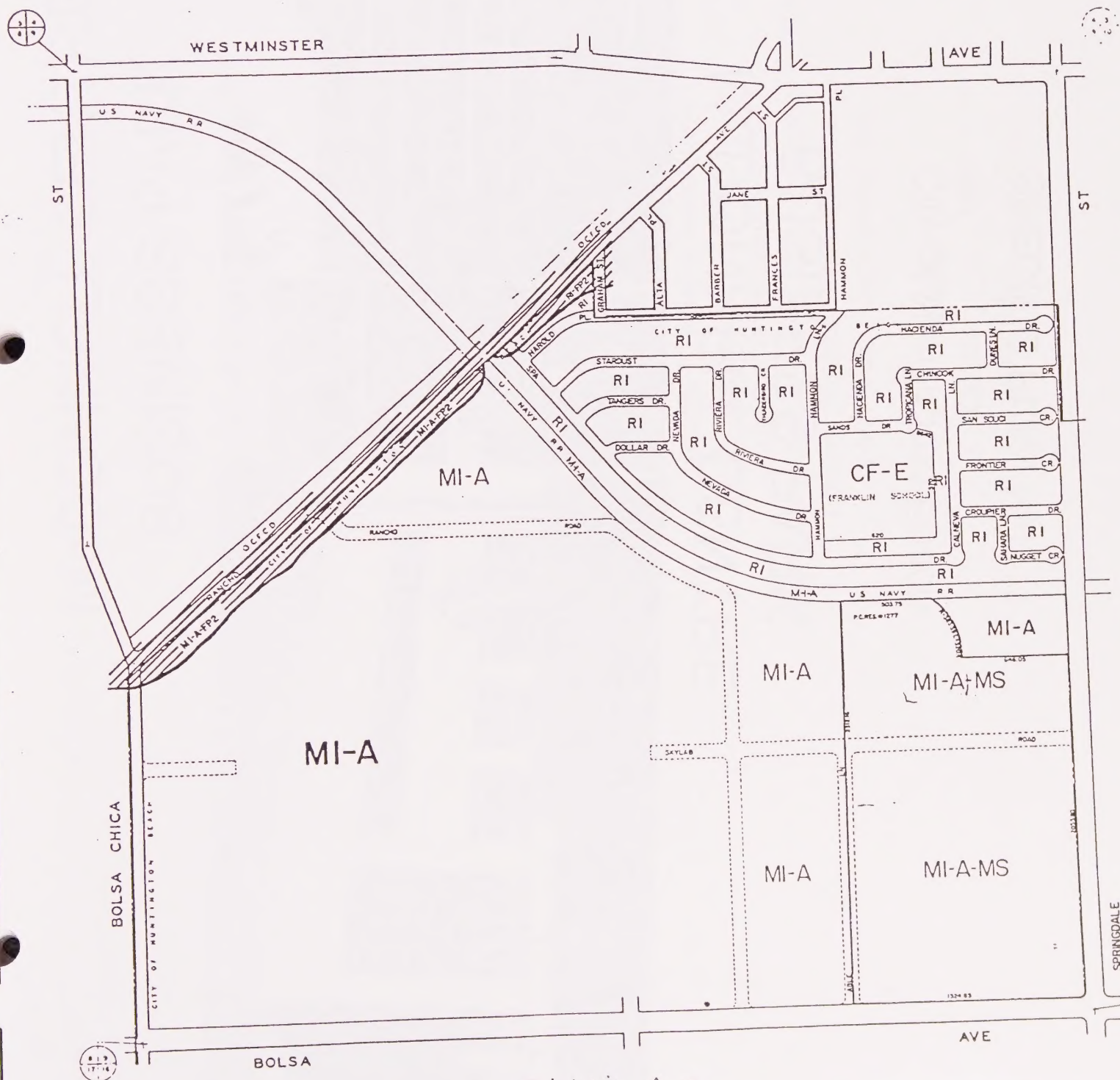
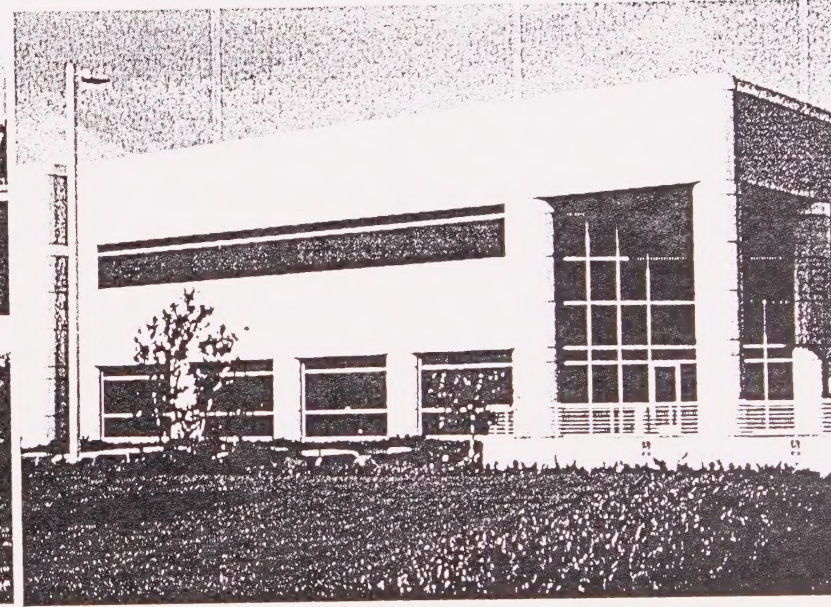


Exhibit A-2



McDONNELL CENTRE BUSINESS PARK

City of Huntington Beach

SPECIFIC PLAN NO. 11

3372

McDONNELL CENTRE BUSINESS PARK

City of Huntington Beach

SPECIFIC PLAN NO. 11



3966

Prepared by
EDAW, Inc. with
Michael C. Adams Associates
Urban Design Camp
Langdon Wilson Architecture
Peter D. Brandow & Associates, Landscape Architects
Adams, Streeter Civil Engineers, Inc.
WPA Traffic Engineering, Inc.

Adopted
October____, 1997
Ordinance No. 3372

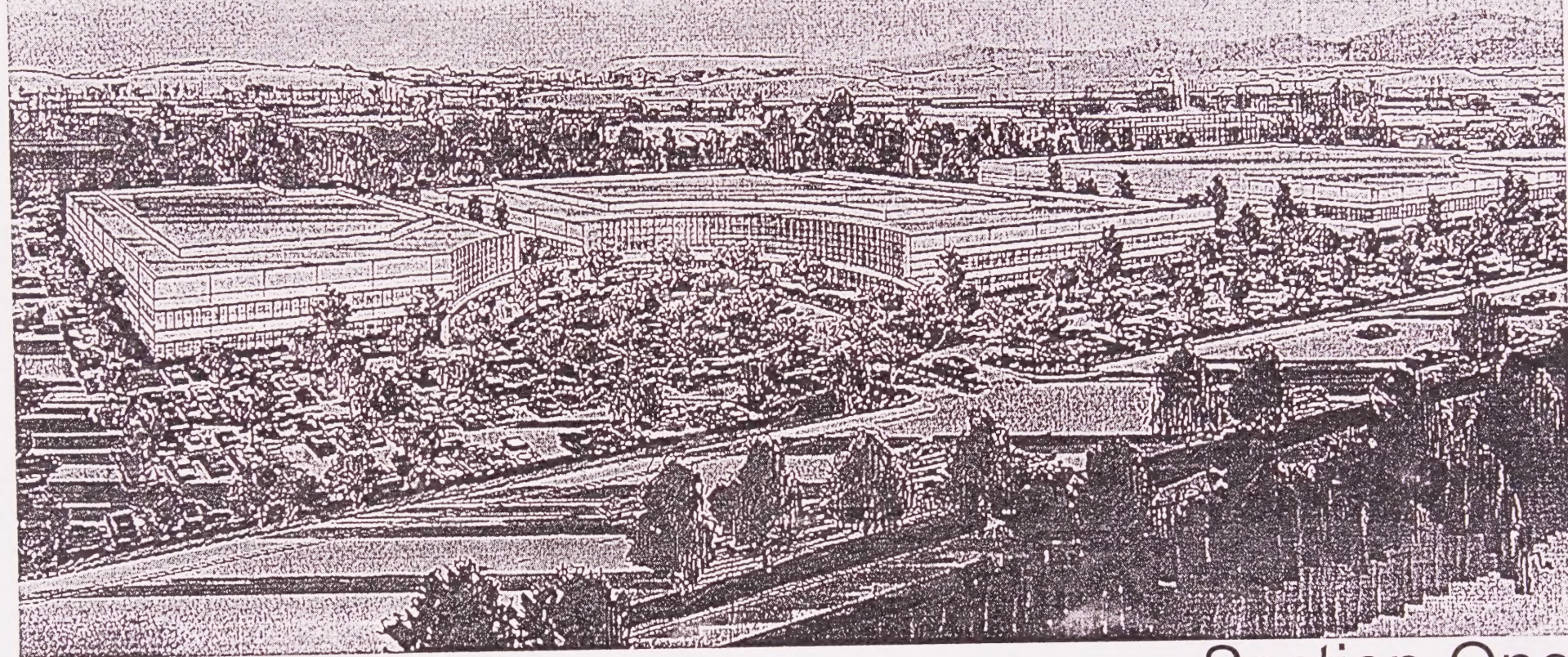
CONTENTS

	<u>Page</u>		<u>Page</u>
1. INTRODUCTION		4. MASTER PLAN CONCEPT	
1.0 Purpose and Intent	2	4.0 Development Concept	21
1.1 Authority and Procedure	2	4.1 Land Use Plan	25
1.2 Scope and Format	3	4.2 Circulation Plan	29
2. PROJECT AREA DESCRIPTION		4.3 Public Facilities Plans	35
2.0 Location	6	4.4 Landscape Concept	43
2.1 General Plan Designation	7	4.5 Phasing Plan	47
2.2 Zoning Regulations	7	5. DESIGN GUIDELINES	
2.3 Current Land Uses	9	5.0 Project Area Character	51
2.4 Planning History	9	5.1 Site Planning Guidelines	51
2.5 State Mandated Requirements	10	5.2 Architectural Guidelines	57
3. IMPLEMENTATION		5.3 Landscape Guidelines	63
3.0 Administration	13	5.4 Signage Guidelines	75
3.1 Development Phasing Plan	13	6. DEVELOPMENT REGULATIONS	
3.2 Methods and Procedures	14	6.0 Purpose	85
3.3 Master Plan	14	6.1 General Provisions	85
3.4 Parcel Maps	14	6.2 Definitions	86
3.5 Site Plan Review	14	6.3 Development Standards	91
3.6 Reuse/Change of Use Review	17	APPENDICES (Volume Two)	
3.7 Environmental Determination	17	A - Legal Description	
3.8 Request for Deviation	17	B - General Plan Consistency	
3.9 Specific Plan Amendments	18	C - Environmental Mitigation Measures	
3.10 Severability	18		

LIST OF EXHIBITS

		<u>Page</u>			<u>Page</u>
Exhibit 1	Vicinity Maps	6	Exhibit 28	Building Corner Treatment	59
Exhibit 2	Aerial Photograph	8	Exhibit 29	Building Materials Variety	60
Exhibit 3	Existing Conditions Map	12	Exhibit 30	Commercial Facade	61
Exhibit 4	Site Plan Review Process (Chart)	16	Exhibit 31	Office Facade	61
Exhibit 5	Illustrative Conceptual Master Plan	20	Exhibit 32	Preservation of Existing Trees	62
Exhibit 6	Land Use Plan	23	Exhibit 33	Typical Parkway Landscaping	64
Exhibit 7	Statistical Analysis (Chart)	24	Exhibit 34	Typical Parking Lot Landscaping	66
Exhibit 8	Land Use Summary (Charts)	26	Exhibit 35	Preservation of Existing Landscaping	68
Exhibit 9	Circulation Plan	28	Exhibit 36	Typical Landscape Buffer	70
Exhibit 10	Street Cross Sections	30	Exhibit 37	Typical Landscape/Parking Lot Buffer	71
Exhibit 11	Private Drive Entry	32	Exhibit 38	Plant Materials Palette	72
Exhibit 12	Water System Plan	34	Exhibit 39	Typical Signage Layout	74
Exhibit 13	Sewer System Plan	36	Exhibit 40	Wall Sign Chart	75
Exhibit 14	Storm Drainage Plan	38	Exhibit 41	Wall Sign Detail	76
Exhibit 15	Landscape Concept Plan	42	Exhibit 42	Entry Wall Sign	77
Exhibit 16	Landscaping on Perimeter Streets	44	Exhibit 43	Freestanding Sign Chart	77
Exhibit 17	Pedestrian Walkway	45	Exhibit 44	Freestanding Sign Detail	78
Exhibit 18	Phasing Plan Map	46	Exhibit 45	Entry Sign Detail	79
Exhibit 19	Infrastructure Improvements (Chart)	48	Exhibit 46	Business Directory Sign	80
	Schedule of Improvements (Chart)	48	Exhibit 47	Information/Directional Sign	80
Exhibit 20	Typical Site Layout	50	Exhibit 48	Temporary Sign Detail	81
Exhibit 21	Screening of Loading & Storage Areas	52	Exhibit 49	Centre Identification Signs	82
Exhibit 22	Shared Reciprocal Access	54	Exhibit 50	Planning Areas & Subareas Map	84
Exhibit 23	Typical Entry Drive	55	Exhibit 51	Permitted Uses (Chart)	90
Exhibit 24	Architectural Details	56	Exhibit 52	Development Regulations Chart	92
Exhibit 25	Building Articulation	57	Exhibit 53	Parking Requirements	93
Exhibit 26	Typical Building Entry	58	Exhibit 54	Development Regulations Check List	95
Exhibit 27	Typical Building Wall	59	Exhibit 55	Design Guidelines Check List	96

INTRODUCTION



Section One

1.0 PURPOSE AND INTENT

The McDonnell Centre Business Park Specific Plan establishes the planning concept, design theme, development regulations and administrative procedures necessary to achieve an orderly and compatible development of the project area; and to implement the goals, policies, and objectives of the Huntington Beach General Plan. The intent is to establish a public private partnership to enable the creation of a "Campus-Park" industrial setting and achieve a high level quality in individual building design.

The McDonnell Centre Business Park Specific Plan identifies the location, character and intensities of the planned business park development activities. The Specific Plan establishes the alignment and design of a circulation system, and all public facilities and infrastructure necessary to facilitate a master planned development. The Specific Plan creates a compatible design theme for the project area and establishes the development regulations necessary to accomplish the identified objectives.

The Specific Plan is regulatory in nature and serves as zoning for the McDonnell Centre Business Park area. Subsequent development plans, Parcel Maps and other entitlement requests for the project area must be consistent with both the Specific Plan and the Huntington Beach General Plan. An Environmental Impact Report (E.I.R. 96-1), with identified mitigation measures, has been prepared as a companion report to the Specific Plan.

1.1 AUTHORITY AND PROCEDURE

The State of California requires that all cities and counties prepare and adopt a comprehensive General Plan for the physical development of their area of jurisdiction. Following the adoption of the General Plan, the entity is required to develop and adopt regulating programs (zoning and subdivision ordinances, building and housing codes, and other regulations), which will implement the policies described in the General Plan.

California State law authorizes cities with complete General Plans to prepare and adopt Specific Plans (Government Code Sections 65450 et. seq.). Specific Plans are intended to be a bridge between the local General Plan and individual development proposals. Specific Plans contain both planning policies and regulations, and may combine zoning regulations, capital improvement programs, detailed development standards and other regulatory methods into one document which can be tailored to meet the needs of a specific area.

Local planning agencies or their legislative bodies may designate areas within their jurisdiction as ones for which a Specific Plan is "necessary or convenient" (Government Code Section 65451).



A Specific Plan may either be adopted by ordinance or resolution (Government Code Section 65507). Adoption by ordinance is common when the Specific Plan amends a development code, zoning ordinance, or other code, when specific regulatory measures are included and when local charters require adoption by ordinance. Resolutions are commonly used when the plan is more of a policy document. Should the legislative body wish to change a proposed Specific Plan recommended by the Planning Commission, the change must first be referred back to the Commission for consideration, if not previously considered (Government Code Section 65504).

Adoption or amendment of a Specific Plan constitutes a project under the California Environmental Quality Act (CEQA) and the State's Environmental Impact Report (EIR) guidelines. If the initial environmental review shows that the proposed or amended plan could significantly affect the environment, the jurisdiction must prepare an EIR and submit it in draft form for public review. The need for an EIR in a particular case is determined by the local government. A Specific Plan and an EIR on a Specific Plan overlap extensively; they must address many of the same concerns and the process for preparing them is nearly identical. Therefore, environmental assessment should be an integral part of preparing or revising a Specific Plan.

The preparation, adoption and implementation of the McDonnell Centre Business Park Specific Plan by the City of Huntington Beach is authorized by the California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457.

The Huntington Beach General Plan has recently been rewritten and adopted by the City Council (May 13, 1996). The amended General Plan maintains the industrial designation for the project area. The McDonnell Centre Business Park Specific Plan is consistent with the goals and policies of the Huntington Beach General Plan.

1.2 SCOPE AND FORMAT

The McDonnell Centre Business Park Specific Plan is divided into six sequential sections. Section One is the Introduction and describes the purpose and intent of the document along with a brief explanation of Specific Plan procedures and authorization.

Section Two is the Project Area Description and is intended to establish the reasons why the Specific Plan process is logical and necessary for this portion of the City. This section presents a general description of the Specific Plan area including designated land uses, existing zoning and existing uses. In addition, special characteristics and existing conditions which make this area unique have been identified.



Section Three is Implementation and discusses the process by which individual projects will be reviewed and approved. This section outlines the Parcel Map and Site Plan Review procedures. This section also describes the process for project appeals and the methods by which the Specific Plan can be modified or amended.

Section Four describes the Master Plan Concept. The design concept evolves from the objectives outlined in Section One and the existing conditions discussed in Section Two, along with input from numerous public meetings and special studies conducted by and for the City. This section presents the development options proposed for various sites and the circulation, public facilities, infrastructure and landscaping which will support the development concept and reinforce the design theme. In addition, a development phasing plan has been prepared as part of this section.

Section Five establishes Design Guidelines for the entire project area and for individual project development. This section identifies and describes the intended character for the area and provides a framework for project implementation.

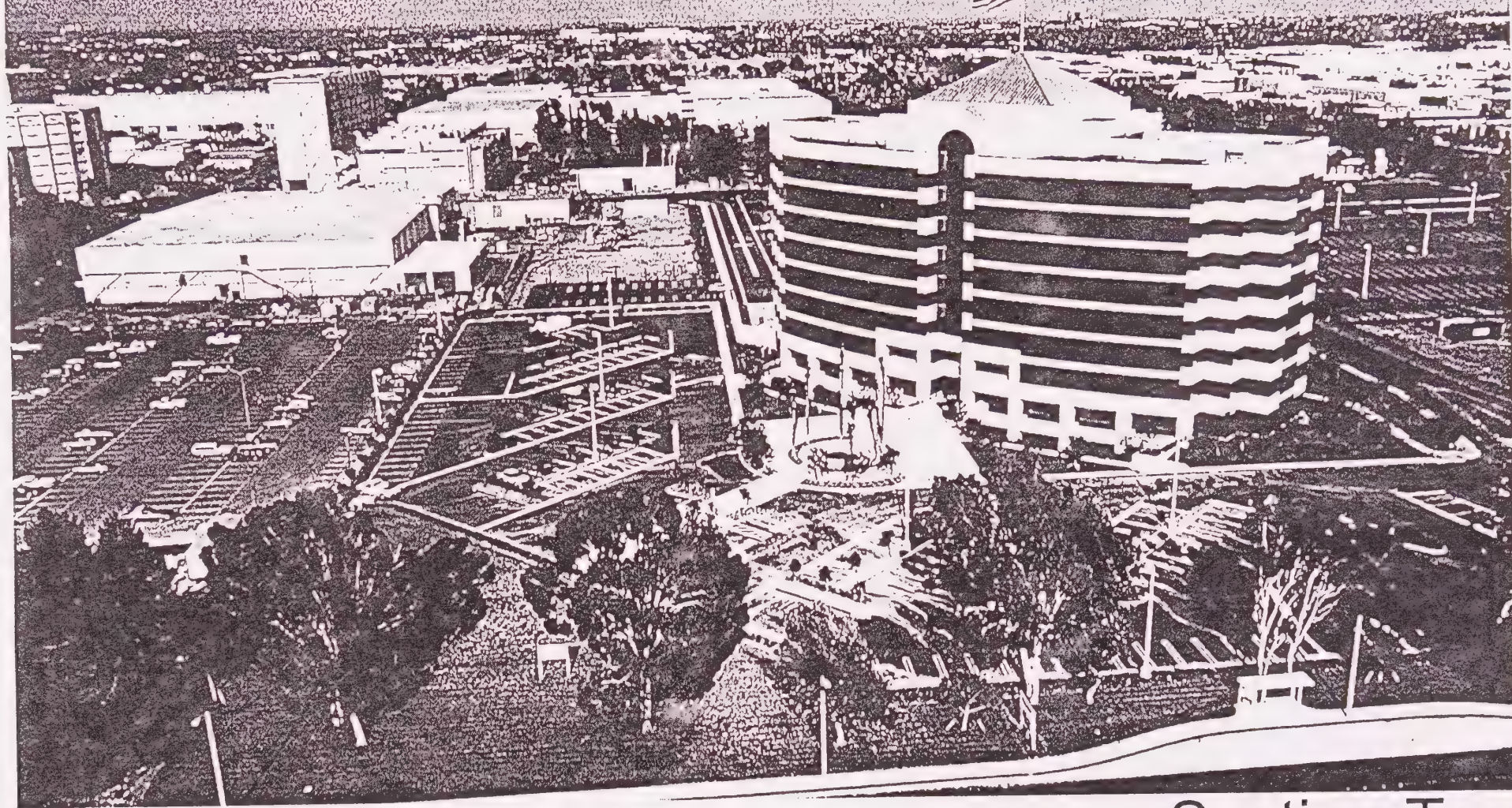
Section Six presents a detailed description of the Development Regulations which are necessary to guide and control new projects and carry out the goals and policies of the Specific Plan and the City's General Plan.



An Appendix (printed under separate cover) has been compiled which contains all the special studies and reports which have contributed to the formation of the Specific Plan. The Appendix (Volume Two) includes the Legal Description of the site, a General Plan Consistency Analysis and identifies the Mitigation Measures desired in the Environmental Impact Report.



PROJECT AREA DESCRIPTION



Section Two



PROJECT AREA DESCRIPTION

2.0 LOCATION

The McDonnell Centre Business Park Specific Plan covers 307 gross acres located in the northwestern portion of the City of Huntington Beach. The area is generally bounded on the north by Rancho Road and the U.S. Navy railroad right-of-way (excluding the City's water reservoir site); on the east by Springdale Street, on the south by Bolsa Avenue and on the west by Bolsa Chica Street. A legal description of properties in the Specific Plan project area has been included in the Appendix.

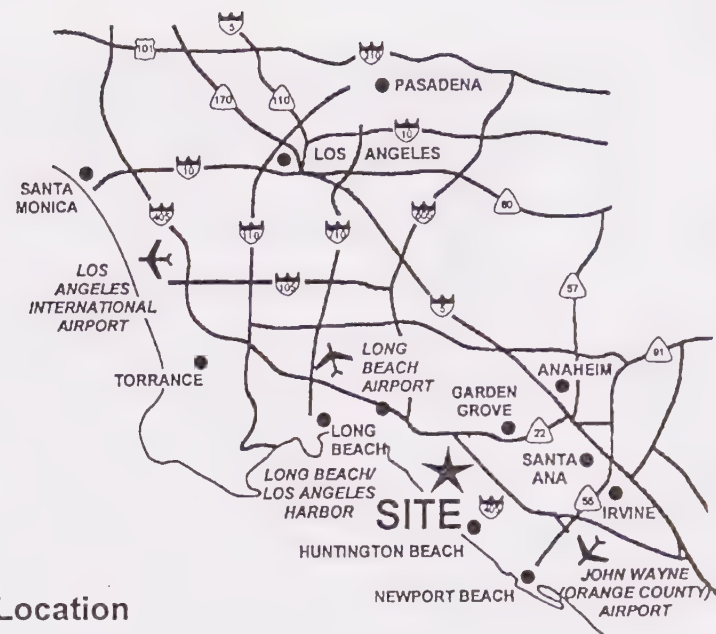


Vicinity Maps

Exhibit 1



Regional Location



The project area is surrounded by a variety of land uses and activities. A railroad spurtrack of the U.S. Navy and Rancho Road create the northern boundary with low density residential uses located across from the railroad tracks (in the City of Huntington Beach) and across Rancho Road (in the City of Westminister). To the south, office and manufacturing uses are located across from Bolsa Avenue. To the east, low density residential and commercial uses are located across from Springdale Street. To the west, is the Orange County Flood Control Channel (CO-3). The property across from Bolsa Chica Street and the flood control channel is owned by the U.S. Navy and is used as part of the Seal Beach Naval Weapons Station (in the City of Seal Beach).

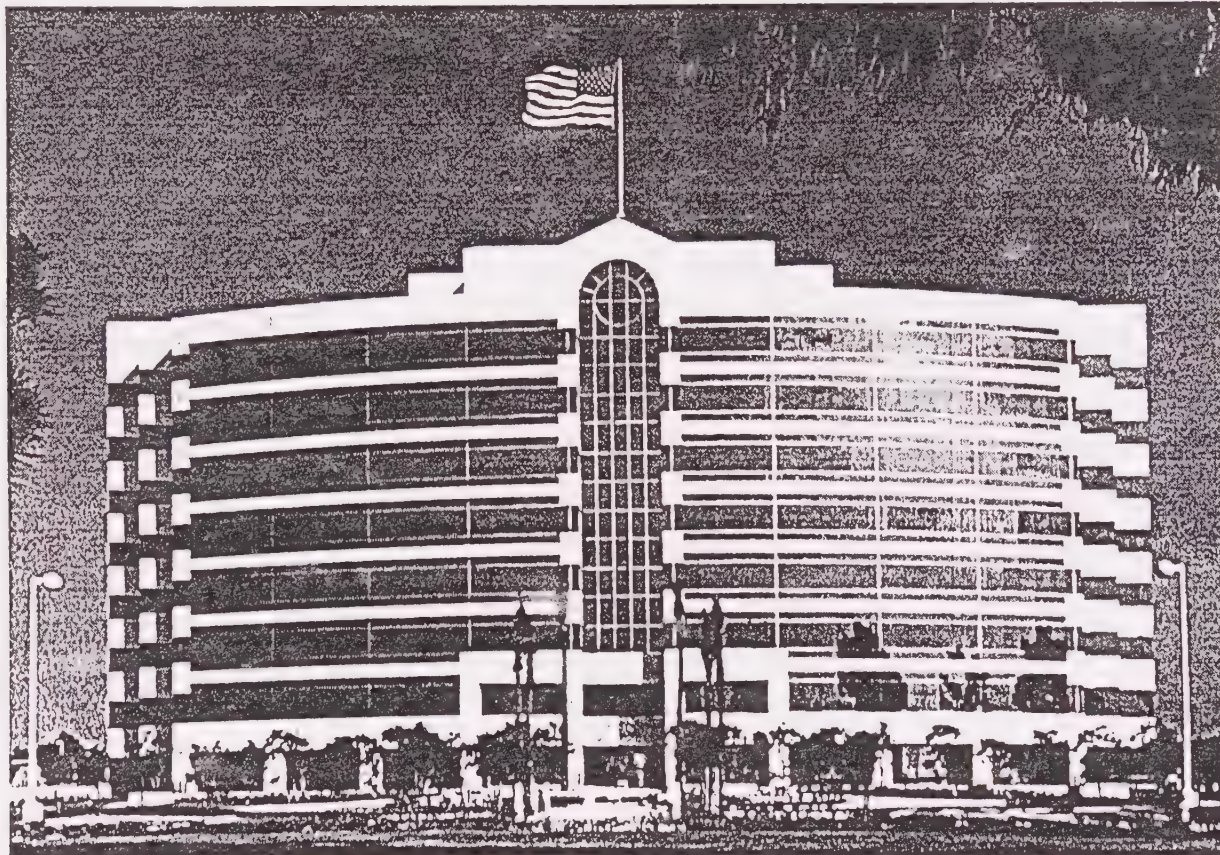


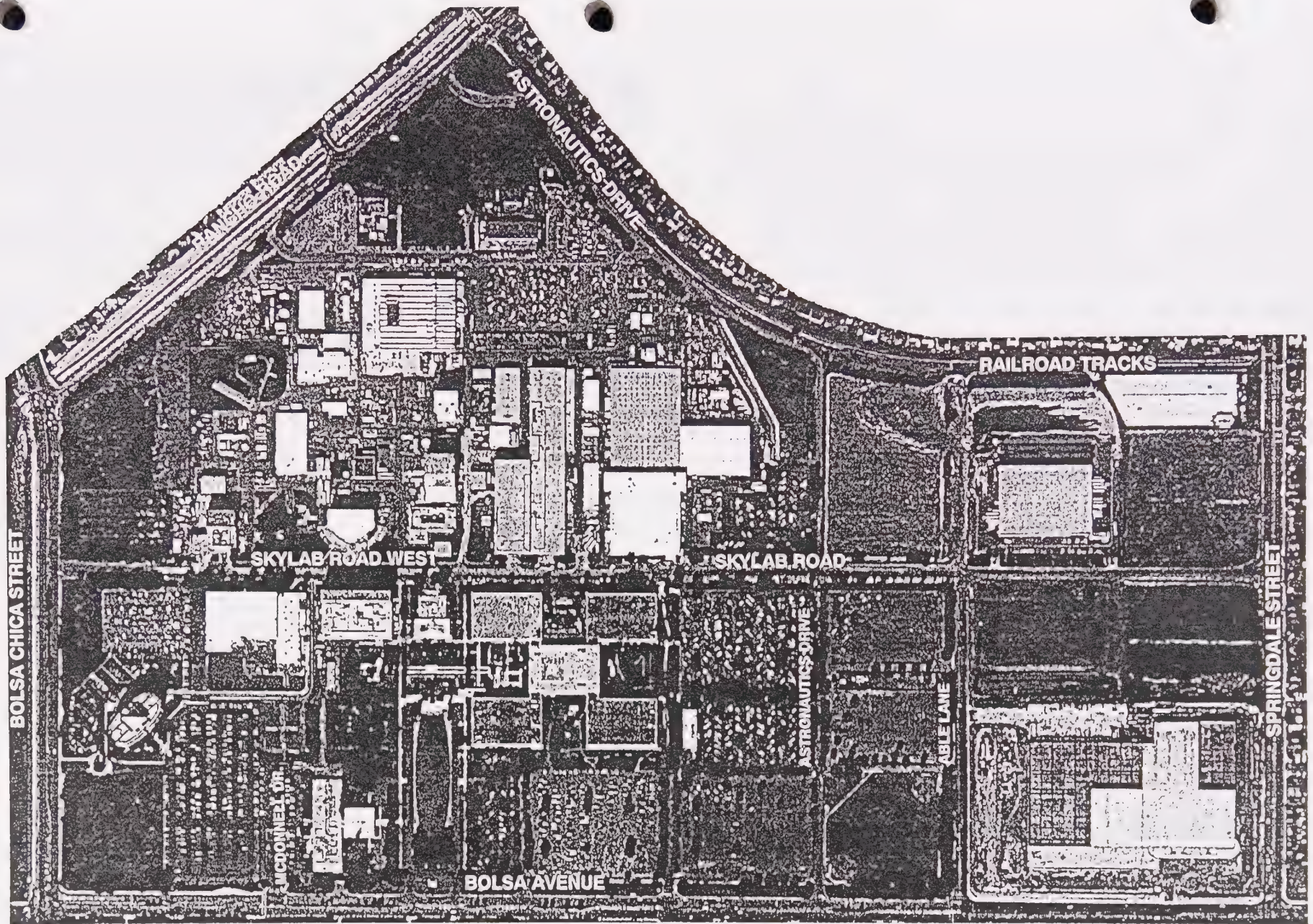
2.1 GENERAL PLAN DESIGNATION

The entire site of the McDonnell Centre Business Park is currently designated as industrial with a floor area ratio (F.A.R.) of 0.75 in City's General Plan. The site has been designated for industrial land uses since the mid 1960's. Continuation of industrial uses in this portion of the City is compatible with the General Plan Goal (LU 12) to "Achieve the development of industrial uses that provide job opportunities for existing and future residents, as well as the surrounding subregion, and generate revenue for the City."

2.2 ZONING REGULATIONS

The McDonnell Centre Business Park is presently zoned limited industrial ("IL") and limited industrial with a high rise overlay ("IL-H"), in designated areas. The adoption of this Specific Plan will supersede the existing zoning and establish a new set of development regulations. The Specific Plan, as presented with industrial, office and support commercial uses, is consistent with the current General Plan; and therefore will not require a General Plan Amendment.





Aerial Photograph

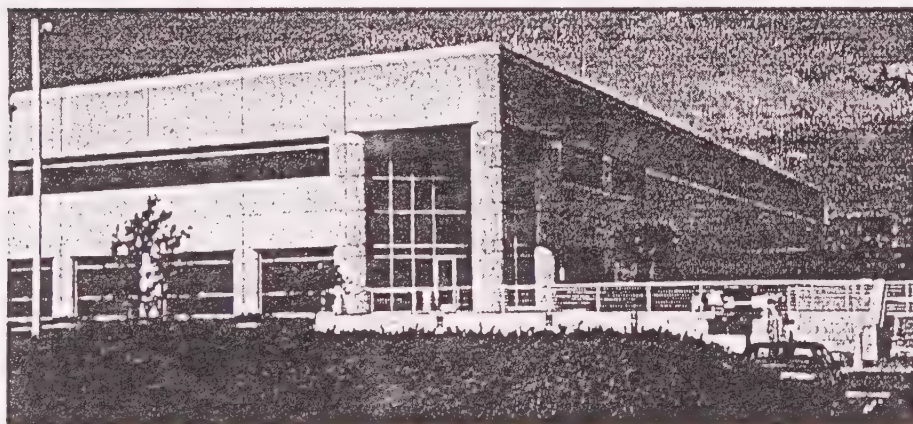
Exhibit 2 - 1997 Aerial Photograph



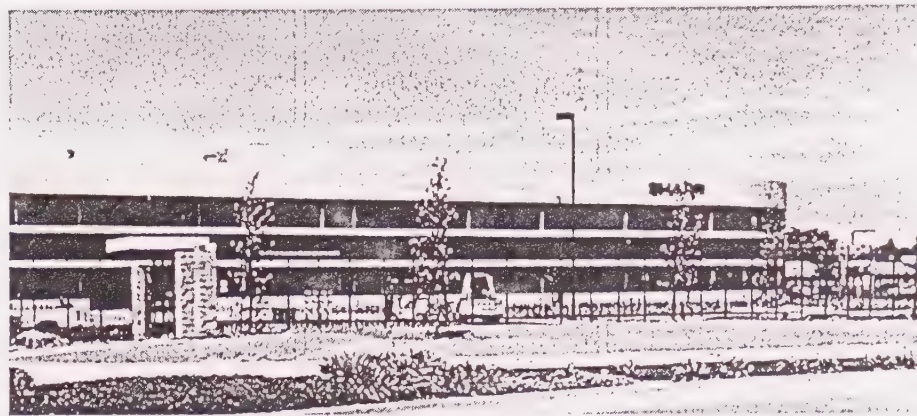
2.3 CURRENT LAND USES

The McDonnell Centre Business Park site currently consists of a number of activities. The primary use is the approximately 2,800,000 square foot McDonnell Douglas Aerospace Facility. The facility consists of numerous industrially related research and development structures along with a high-rise office building. This facility has been approved and developed in numerous phases over many years beginning in 1963.

New private developments are beginning to appear on the eastern portion of the project area, replacing the truck farming operations of the past. Cambro Manufacturing has constructed and occupies a 120,000 square foot facility, with ultimate development of 280,412 square feet. Sharp Electronics is currently constructing a 538,859 square foot facility. The remainder of the Specific Plan area is vacant, with the exception of a network of roadways and remote parking facilities. Portions of the project area are currently entitled and many parcels are being actively marketed for new industrial and commercial development. The activities described in the Specific Plan reflect the beginning of the planning process (June 1996). Since the initiation of this exercise



a number of projects have been reviewed and approved subject to the draft specifications of the Specific Plan.



2.4 PLANNING HISTORY

The McDonnell Centre Business Park project area has been the subject of many special studies with a variety of development options explored. In 1981 a proposal was prepared for the development of 1,200,000 square feet of industrial and office space. The proposal was for a Restricted Manufacturing Zone Change which allowed for an "appropriate" mix of uses; concurrent with the Zone Change request, a Multi-Story designation was also requested. The Industrial/Office complex plans consisted of industrial and warehouse buildings, office buildings, a hotel and restaurant with a parking structure, interior circulation system and landscaped open space. The office and hotel structures were proposed for six and seven stories along the eastern portion of the project area adjacent to Springdale Street. Due to market conditions, the project was not pursued.

2.5 STATE MANDATED REQUIREMENTS

To comply with the State of California legislated mandates, the City of Huntington Beach has adopted several plans to deal with regional issues including Air Quality, Congestion Management, Growth Management and Transportation Demand Management Plans.

2.5.0 POLICY :

All projects within the Specific Plan area shall comply with the applicable provisions of the following plans:

2.5.1 South Coast Air Quality Management Plan

The South Coast Air Quality Management Plan (AQMP) requires measures to reduce traffic congestion, improve air quality, and requires that cities develop Air Quality components within their General Plans. These measures include Regulation XV, a program which requires employers of more than one hundred (100) persons to prepare trip reduction plans, and a requirement for jurisdictions to prepare an air quality component in the General Plan.

The City of Huntington Beach is subject to all local jurisdiction requirements set forth by the AQMP. The City has adopted an Air Quality Element which incorporates AQMP measures.

2.5.2 Congestion Management Plan

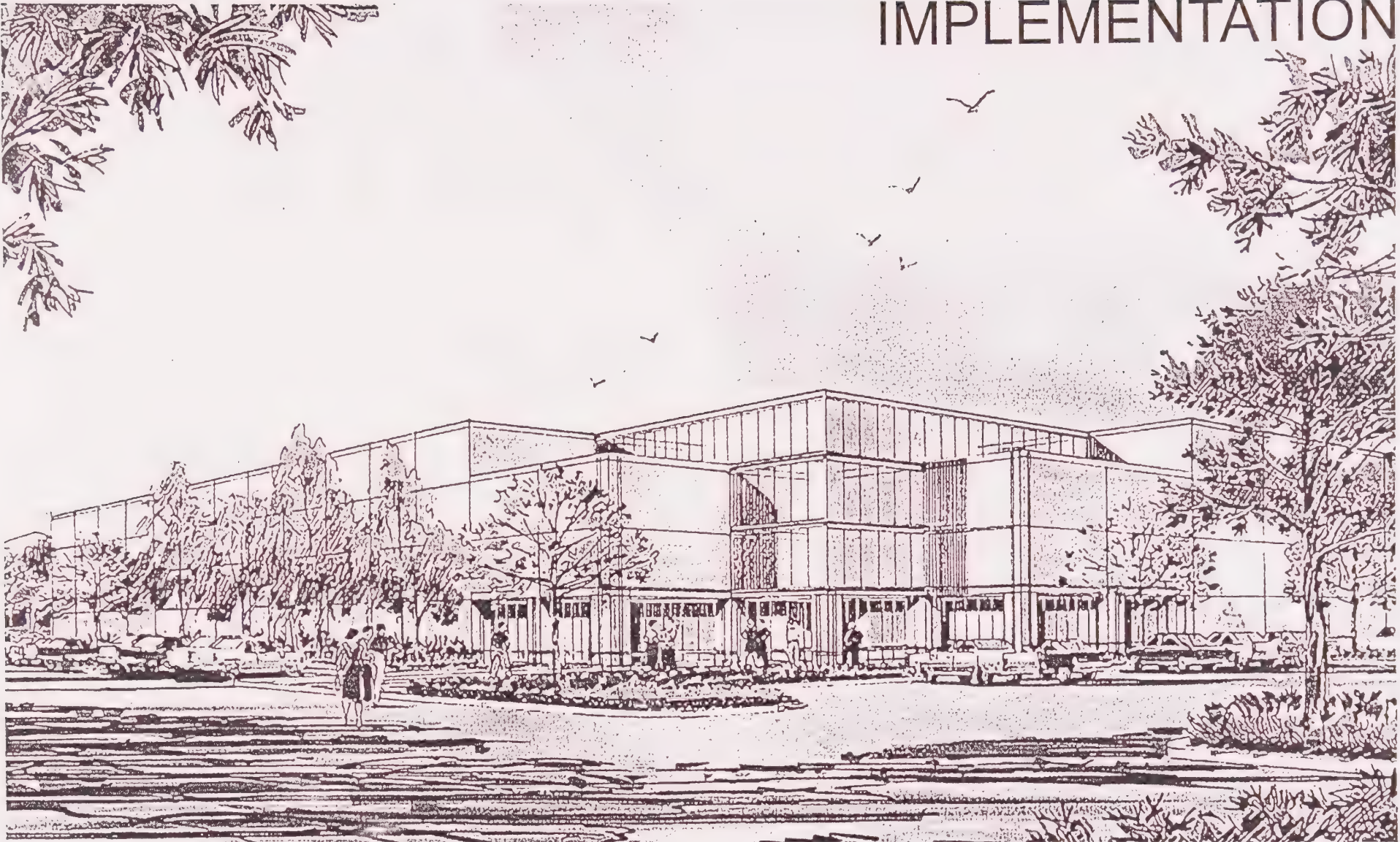
The Congestion Management Plan (CMP) is required by Assembly Bill 471 (Proposition 111), subsequently modified by Assembly Bill 1791. This Bill requires every urbanized county to adopt a CMP; the County of Orange has prepared a CMP which includes the City of Huntington Beach. The CMP requires mitigation of traffic impacts of development, as well as trip reduction programs. The City of Huntington Beach has completed the mandated components of the CMP including level of service standards, trip reduction program, and a capital improvements program for traffic and transit.

2.5.3 Growth Management Plan

A Growth Management Plan (GMP) is required to implement the passage of Orange County Measure M approved in the 1990 election. Its purpose is to ensure that the planning, management, and implementation of traffic improvements and public facilities are adequate to meet current and projected needs. The City has an approved Growth Management Element which meets the requirements for Measure M funding and an adopted Transportation Demand Management Ordinance.

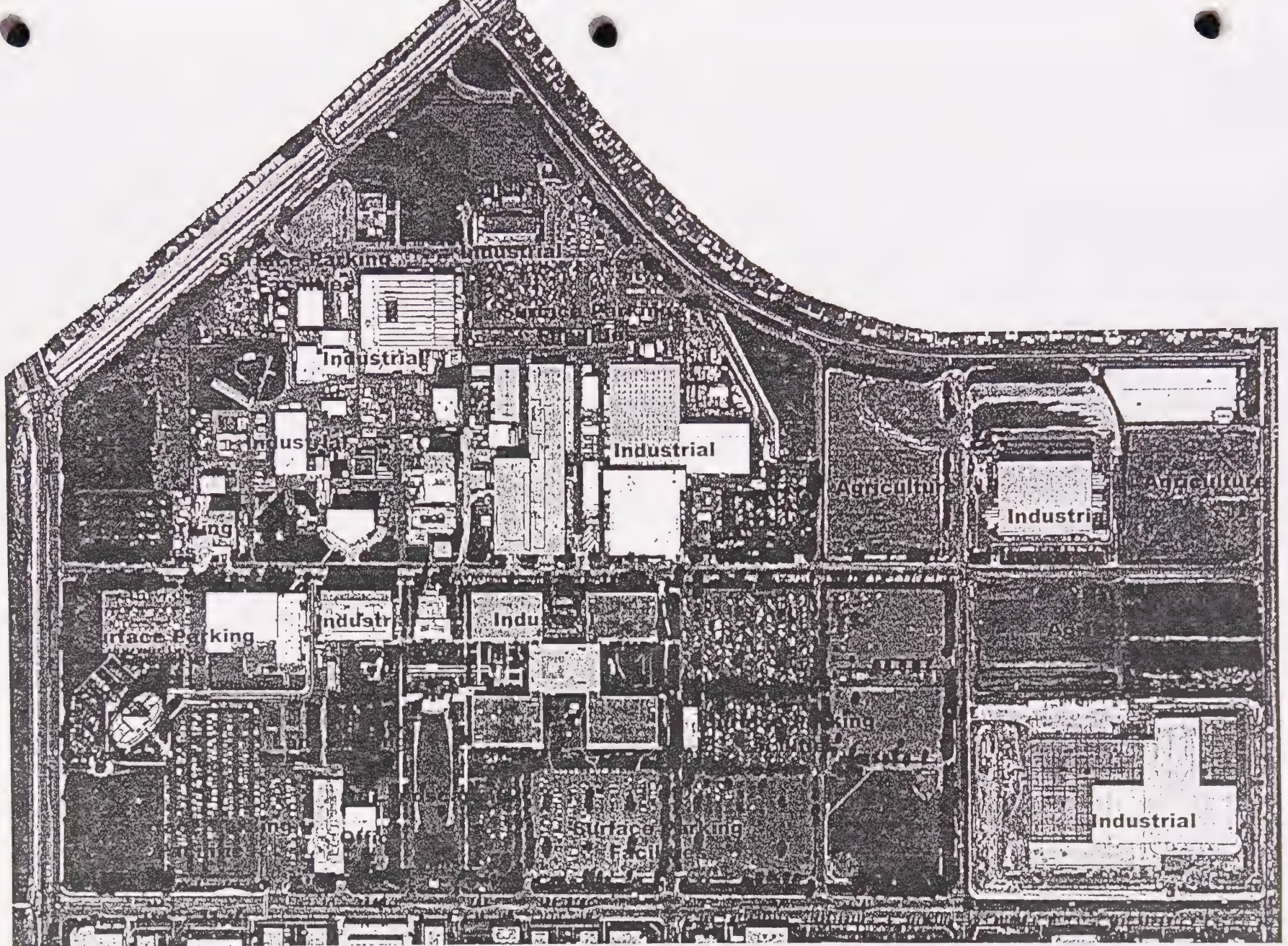
Transportation Demand Management (TDM) measures are generally directed at increasing auto occupancy, decreasing peak hour usage, and managing demand for transportation facilities. The City TDM Ordinance is part of its compliance with the Growth Management Plan.





Section Three





Existing Conditions

Exhibit 3 - 1997 Aerial Photograph



IMPLEMENTATION

3.0 ADMINISTRATION

The City's Director of Community Development shall administer the provisions of the McDonnell Centre Business Park Specific Plan in accordance with the State of California Government Code, Subdivision Map Act, the Huntington Beach Municipal Code, and the City's General Plan.

The Specific Plan development procedures, regulations, standards and specifications shall supersede the relevant provisions of the City's Zoning Code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Any development regulation and building requirement not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of an individual request.

The Director of Community Development shall have the discretion to determine if requests for modification to the Specific Plan are minor or major. Minor modifications or amendments may be accomplished administratively by the Director. Major amendments will require the processing of a Zoning Text Amendment, subject to the City's processing regulations in place at the time of the request. Minor modifications to the Specific Plan include, but are not limited to:

- The addition of information to the exhibits or text which serve to clarify, but do not change the meaning or intent.

- Changes to the infrastructure (i.e., storm drain, water and sewer systems), as recommended by the City's Director of Public Works.
- The adjustment, addition and/or lot consolidation as addressed in this section of the Specific Plan.
- Modifications to the alignment of the Planning Area boundaries to coincide with specific development plans, as recommended by the City's Community Development Director.

All modifications must be reviewed for compliance with the goals and policies of the City's General Plan, intent of the McDonnell Business Park Specific Plan and consistency with the Environmental Impact Report.

3.1 DEVELOPMENT PHASING PLAN

The McDonnell Business Park Specific Plan area is designed for maximum flexibility and anticipates that individual development projects will be constructed over a period of years; with no specific target date for completion. Development starts and occupancy will be dictated by market forces and phased accordingly.

A development Phasing Plan (Section 4.5) has been prepared identifying a program of the relative timing of development within each of the Planning Areas. The Phasing Plan also provides a guideline for the construction of community infrastructure and public improvements to adequately service new projects within the Specific Plan area.



3.2 METHODS AND PROCEDURES

The methods and procedures for implementation of the Specific Plan shall be on a project by project basis. The adoption of the Specific Plan alone will not require infrastructure improvements to the project area. Physical improvements will only coincide with the recordation of a Parcel Map and/or approval of an individual development project. The Specific Plan is a regulatory document and is not intended to be a Development Agreement.

3.3 MASTER PLAN

A Master Plan Concept for the project area identifying primary and alternative land uses, circulation system, infrastructure layout, public facilities and landscape scheme has been prepared (Section Four) in conjunction with the Specific Plan. All proposed projects shall be consistent with the intent of the Master Plan Concept.

3.4 PARCEL MAPS

The project area will be subdivided through a series of Parcel Maps. Parcel Maps shall be prepared consistent with the Master Plan Concept to create developable sites. These maps shall identify the infrastructure and improvements necessary to support the anticipated projects, subject to review by the City's Public Works and Fire Departments.

Upon recordation, Parcel Maps may be further divided and/or adjusted by filing a subsequent Parcel Map or a Lot Line Adjustment, pursuant to the provisions of the Subdivision Map Act. A parcel may be created with or without a specific project plan. A Parcel Map may be approved, or conditionally approved by the Director of

Community Development and City Engineer providing the proposal is found to be in compliance with the Specific Plan.

In order to approve a Parcel Map the Director of Community Development shall make the following findings:

- The Parcel Map is consistent with the General Plan Land Use Element designation of Industrial (F.A.R. 0.75), the Specific Plan, and all other applicable provisions of the City's adopted codes and regulations; and
- The site is physically suitable for the type and density of development proposed; and
- The design of the subdivision or the proposed improvements will not cause serious health problems or substantial environmental damage or substantially and avoidably injure fish or wildlife or their habitat; and
- The design of the subdivision or the type of improvements will not conflict with easements, acquired by the public at large, for access through the use of property within the proposed subdivision unless alternative easements, for access or for use, will be provided.

3.5 SITE PLAN REVIEW

Individual development projects within the McDonnell Centre Business Park Specific Plan project area shall be



implemented through a Site Plan Review process. A Site Plan Review shall be required for all new development activity, with the exception of interior improvements, general maintenance and repair or other minor construction activities that do not result in an intensification of the use. These exceptions may be subject to other Building and Public Works permits and approvals prior to commencement.

A Site Plan Review request shall be submitted to the City on a standard City application form. Prior to the submittal to the City; McDonnell Douglas Realty Company or their successors in interest may review all plans for acceptability of design, compliance with the CC&R's and Specific Plan intent.

Application to the City for a Site Plan Review shall include a narrative of the proposed activity along with preliminary development plans and drawings. The narrative shall consist of a project description identifying the intended services offered with square feet, hours and days of operation, number of employees, and other information as appropriate. Supplemental to the application submission, project plans shall be prepared including the following preliminary plans: site plan, floor plans, elevations, landscaping, grading, fencing and signage plans; other plans may be required depending on the complexity of the project. The entire parcel shall be plotted with dimensions and all pertinent data and include dimensions to the nearest intersecting public street and identify all street names. In addition, all existing and proposed physical features and structures on the subject property and abutting properties shall be plotted.

The application shall also include a legal description of the property, identification of the uses for each room on

the floor plans and a list of all the building materials, and exterior colors. An application fee for this service shall be established by a separate resolution of the City Council.

The Director of Community Development has the authority to approve, conditionally approve, or deny a Site Plan Review. A Site Plan Review application may also require analysis and comments from various departments of the City. In order to approve a Site Plan Review application, the Director of Community Development shall make the following findings:

- The request is consistent with the City's General Plan and all applicable requirements of the Municipal Code; and
- The requested activity will not be detrimental to the general welfare of persons working or residing in the vicinity nor detrimental to the value of the property and improvements in the neighborhood; and
- The requested activity will not adversely affect the Circulation Plan (Exhibit 9); and
- The requested activity will comply with the provisions of the McDonnell Centre Business Park Specific Plan and other applicable regulations or special conditions required of the project.

The action of the Director of Community Development shall be final unless appealed to the Zoning Administrator by the applicant within ten calendar days of written notification. Such Appeals for a Site Plan Review shall be subject to the procedures outlined in the City's Zoning and Subdivision Ordinance.

Site Plan Review Process

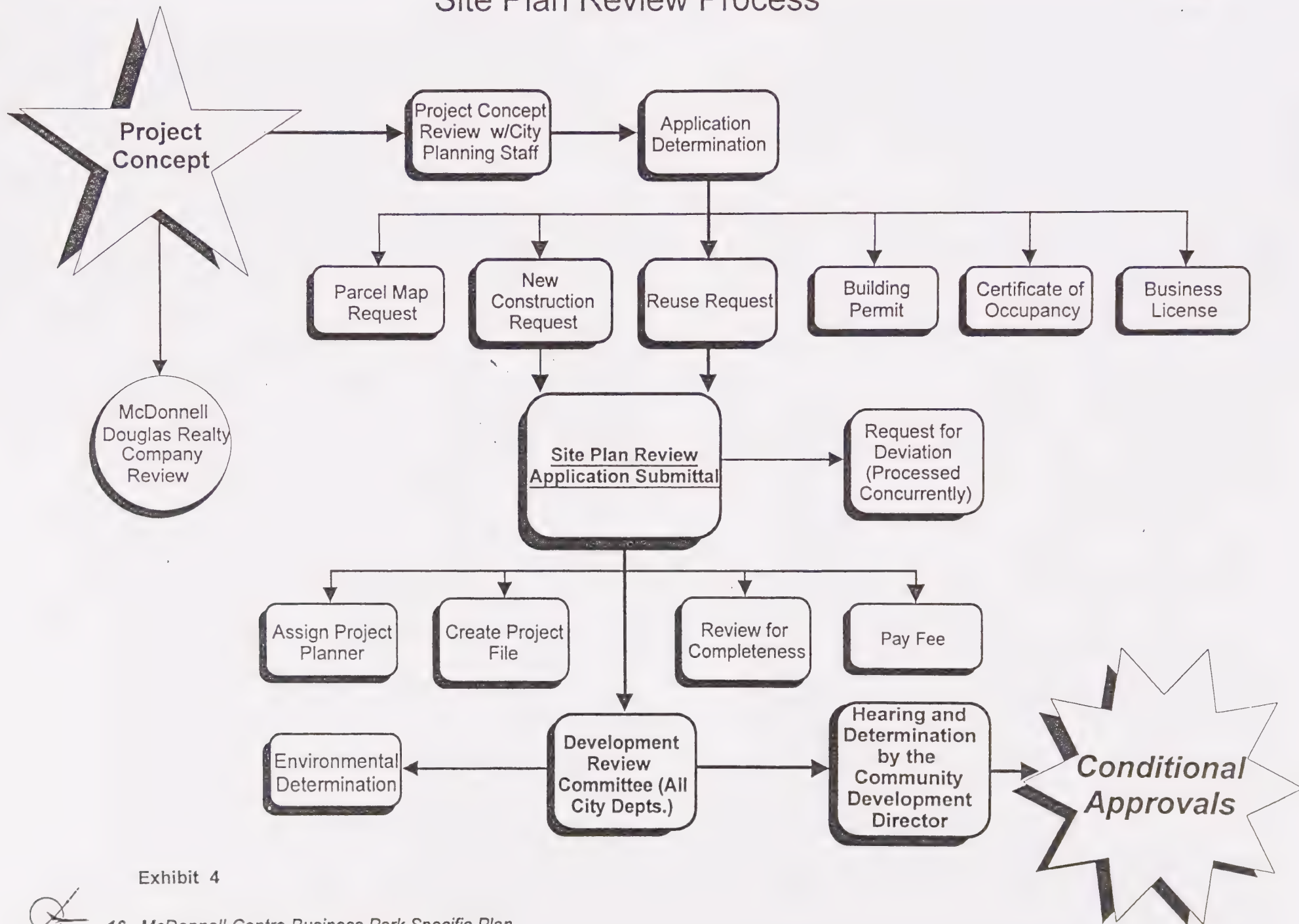


Exhibit 4

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A Site Plan Review approval shall be valid for a period of one year. Additional one year extensions may be requested for a maximum of two years. Such an extension request must be made in writing by the original applicant, property owners, and/or authorized designee, a minimum of thirty days prior to the expiration of the current approval. If construction activity does not commence within the approval or extension period, the entitlement shall be terminated. All final decisions on site plan review proposals shall be the responsibility of the Director of Community Development.

3.6 REUSE/ CHANGE OF USE REVIEW

Any proposal to reuse and/or change the use of a previously approved and constructed development, within the project area, will be subject to additional review by the Community Development Department. The additional review will follow the same procedures outlined in the Site Plan Review process. A "like for like" change of use (i.e. office to office or light manufacturing to light manufacturing) may only be subject to the requirements for a new certificate of occupancy; however any intensification of use shall require a new Site Plan Review. The primary concern will be to review the project request with the E.I.R. "Trip Generation Budget" and analyze the on-site ratio of available parking with the anticipated number of employees. In addition any proposed physical modifications to the existing structure and/or site shall be subject to additional review and approval of the Director prior to the issuance of building permits.

3.7 ENVIRONMENTAL DETERMINATION

The extent and intensity of all anticipated development activity for the McDonnell Centre Business Park area has been identified in the Specific Plan and analyzed in Environmental Impact Report 96-1.

Development project requests consistent with the Specific Plan shall not be subject to additional environmental review unless otherwise required by C.E.Q.A. However, the Director of Community Development may request an additional environmental assessment for unique or unusual circumstances, that have not been previously addressed in the environmental review.

The Director of Community Development may impose environmental mitigation measures, as specified in the E.I.R., as conditions of approval on individual Site Plan Reviews. Such conditions of approval shall describe the time period and manner in which the mitigation measure must be satisfied.

3.8 REQUEST FOR DEVIATION

The McDonnell Centre Business Park Specific Plan Development Regulations (Section Six) are intended to encourage projects which create an aesthetically pleasing appearance, enhance the environment, and facilitate innovative quality architectural design with an adaptation to the surrounding environment.

Deviations from the Development Regulations of the Specific Plan may be granted at the time of Site Plan Review for special circumstances and/or unique architectural features. Requests for Deviation may include but are not limited to parcel size, building height,



3372

site coverage, setbacks, open space, parking, and landscaping. Deviation requests, up to twenty (20) percent of any single standard, may be considered by the Director of Community Development. Deviations greater than twenty (20) percent must be approved by a Variance application before the Zoning Administrator, subject to the procedures outlined in the City's Zoning and Subdivision Ordinance. Development and construction phasing of selected provisions and features may be approved by the Director concurrent with a Site Plan Review and shall not require a Request for Deviation or Variance to the Specific Plan.

Deviations shall be allowed when, in the opinion of the Director of Community Development, significantly greater benefits from the project can be provided than would occur if all the minimum requirements were met. Some additional benefits which may make a project eligible for consideration include: greater open space, greater setbacks, unique or innovative designs, public open space, and the use of energy conservation or innovative technology. The Director of Community Development may approve the Request for Deviation in whole or in part upon making the following findings:

- Promote better design, environmental and land planning techniques and contribute to the economic viability of the community, through aesthetically pleasing architecture, landscaping and site layout; and
- Not be detrimental to the general health, welfare, safety and convenience of the neighborhood or City in general, nor detrimental or injurious to the value of property or improvements of the neighborhood or of the City in general; and

- Be consistent with objectives of the Specific Plan in achieving a project adapted to the area and compatible with the surrounding environment; and
- Be consistent with the goals and policies of the City's General Plan, and comply with State and Federal Law.

3.9 SPECIFIC PLAN AMENDMENTS

Specific Plan Amendments, other than a Minor Modification as previously described (Section.3.0), shall be made through the Zoning Text Amendment process; subject to consideration and approval of the Planning Commission and City Council in accordance with the provisions of the Huntington Beach Zoning and Subdivision Ordinance. Such Amendments may include changes to the Master Plan Concept, Design Guidelines policies and the introduction of alternative Development Regulations.

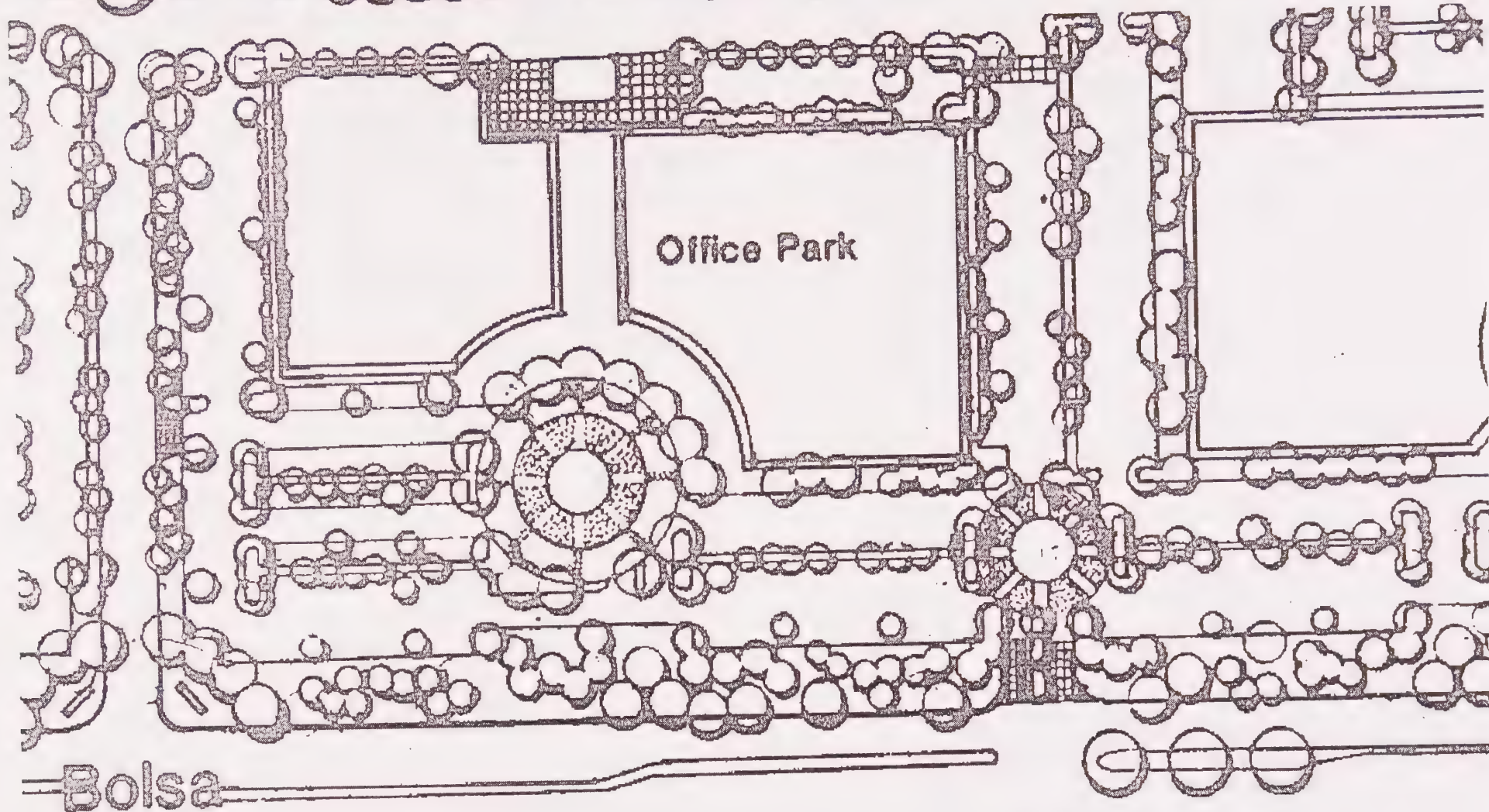
3.10 SEVERABILITY

If any section, subsection, sentence, clause, phrase, or portion of this title, or any future amendments or additions hereto, is for any reason held to be invalid or unconstitutional by the decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this title, or any future amendments or additions hereto. The City hereby declares that it would have adopted these titles and each sentence, subsection, clause, phrase, or portion or any future amendments or additions thereto, irrespective of the fact that any one or more sections, subsections, clauses, phrases, portions or any future amendments or additions thereto may be declared invalid or unconstitutional.



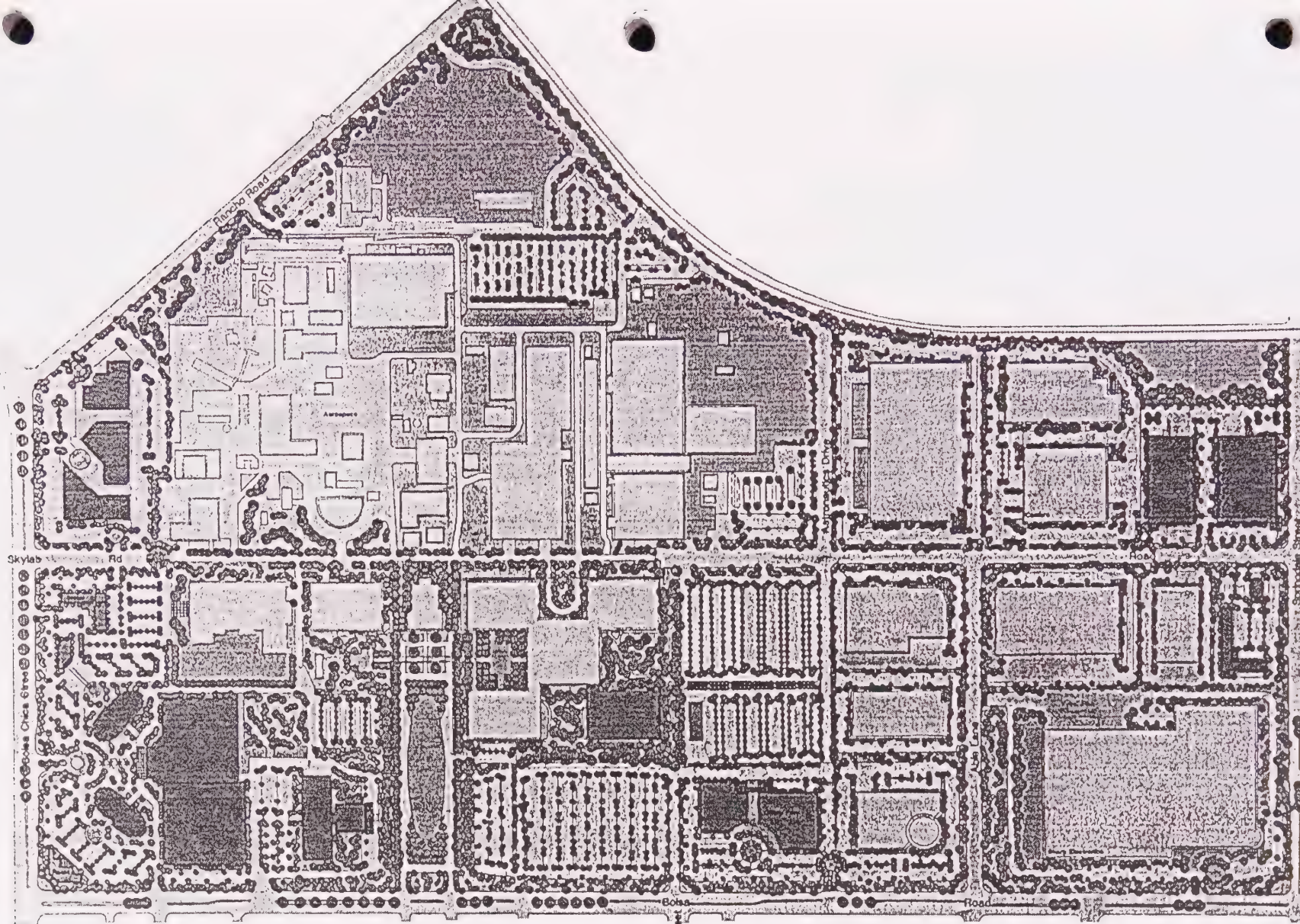
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MASTER PLAN CONCEPT



Section Four





Note: This illustrative shows a hypothetical development scenario on the project site.

Illustrative Conceptual Master Plan

Exhibit 5



MASTER PLAN CONCEPT

4.0 DEVELOPMENT CONCEPT

The McDonnell Centre Business Park Specific Plan development concept provides for a planned Industrial/Business Park complex in the Northwestern portion of the City of Huntington Beach. The Specific Plan establishes the general type, location, parameters and character of all development within the sites boundaries, while allowing for creative design ideas on individual projects consistent with an overall concept.

The McDonnell Centre Business Park will be a large (307 gross acre) master planned industrial facility with office and supporting retail uses. The Specific Plan is designed to allow for development in a manner that is compatible with the surrounding neighborhood and City of Huntington Beach. The area will be incrementally developed in phases over an extended period of time and provide an opportunity for a variety of quality industrial, office and commercial uses, consistent with the City's General Plan.

The McDonnell Centre Business Park Specific Plan provides the framework and guidelines necessary to create a unique, high quality, industrial, research and development business park complex. The site's natural features and proximity to regional transportation systems make

the area ideal for a variety of compatible business land uses and activities. The development concept is designed in concert with the area's history of industrial activities and the community's need for a strong self-sufficient economy.

The McDonnell Centre Business Park provides for a range of employment opportunities in the professional, retail, service and industrial fields; and will widen the employment base of the community. The Specific Plan establishes a clear development concept to assure the facilitation of a cohesive complex. Design measures encompassing site planning, area landscaping, building architecture, streetscapes, pedestrian linkages, setbacks and signage have been established. Concern and adherence to these details will combine to create a unique and integrated business park development.

A conceptual illustrative site plan (Exhibit 5) has been prepared showing a hypothetical scenario utilizing the various guidelines described in the Specific Plan. The plan provides a potential layout identifying building orientation and placement, parking design and access, roadway configuration, entryways and landscaping.

The plan is not intended to reflect an ultimate build-out situation; a large variety of development patterns and activities may evolve consistent with the Specific Plan policies, guidelines and regulations.



The Specific Plan recognizes that development will occur over a long period of time and, therefore, must anticipate future concerns for the area. In order to address this concern, flexibility has been incorporated into the Specific Plan Development Regulations (Section Six). This flexibility in development guidelines is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

The objective of the Specific Plan is to implement the goals and policies of the Huntington Beach General Plan by defining the physical development of the McDonnell Centre Business Park site. Included in this approach are the establishment of land use, circulation, infrastructure, landscape and architectural design characteristics for the project area. The Specific Plan consists of six major components which will guide the development process: the Land Use Plan, Circulation Plan, Public Facilities Plan, Landscape Concept, Design Guidelines, and detailed Development Regulations.

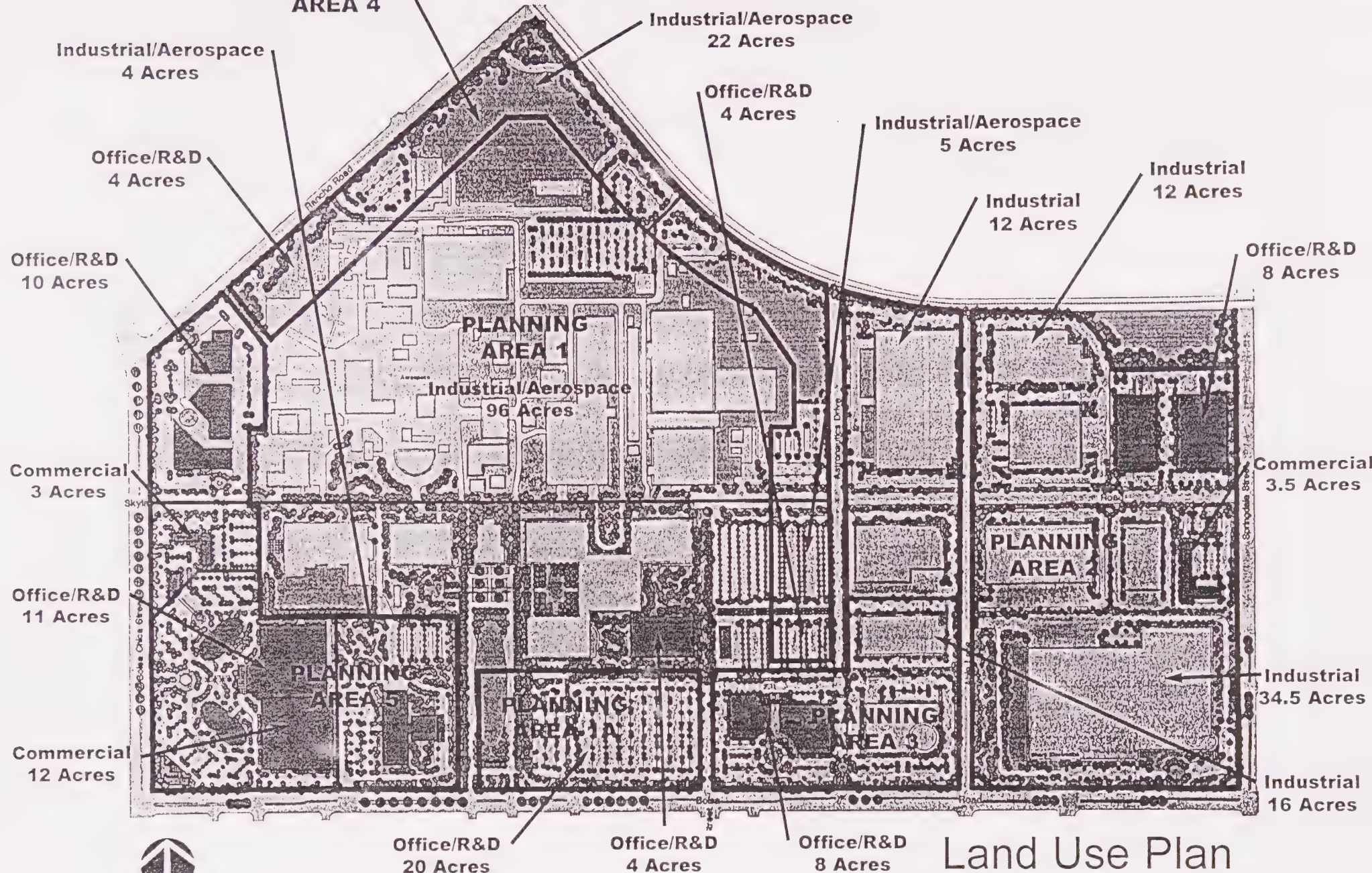
The Land Use Plan addresses the need for flexibility by allowing for a variety of activities within each Planning Area. This approach will enable the existing McDonnell Douglas Aerospace Facility to expand into the undeveloped portions of adjacent Planning Areas or allow for compatible new private development.

The Specific Plan recognizes that the major portion of the project area has been developed around the existing aerospace facility operations. The Specific Plan anticipates a continuation and possible expansion of the aerospace facility. Any modifications and or expansion of the existing aerospace facility shall continue to be regulated by the provisions of the Limited Industrial ("IL") provisions of the Huntington Beach Zoning and Subdivision Ordinance. Any reuse, subdivision and subsequent new non aerospace development shall be subject to the provisions of the Specific Plan.

The Specific Plan identifies and requires sufficient infrastructure and public facilities to adequately and efficiently support any and all anticipated land uses and activities. These improvements will be phased to coincide with or precede individual development projects. This upfront effort will allow future development projects to obtain City approval in an expedited manner, providing the individual projects are consistent with the Specific Plan and the Environmental Impact Report. The McDonnell Centre Business Park Specific Plan identifies effective land planning and design regulations techniques in a format which can take advantage of the best ideas of tomorrow.



PLANNING AREA 4



Note: This illustrative shows a hypothetical development scenario on the project site.

Land Use Plan

Exhibit 6



	Plan Area	Industrial Uses				Office Uses		Commercial Uses			TOTALS
		Aerospace	Manufacturing	Warehousing	Light Industrial	Research Development	Office Park	Retail	Restaurant	Hotel	
Existing Development	1	2,789,053									2,789,053
	1A										0
	2			120,000							120,000
	3										0
	4										0
	5						235,831				235,831
Subtotal		2,789,053	0	120,000	0	0	235,831	0	0	0	3,144,884

Entitled Projects	1										0
	1A										0
	2		110,400	490,732			98,139				699,271
	3										0
	4										0
	5						345,551	9,600	14,000		369,151
Subtotal		0	110,400	490,732	0	0	443,690	9,600	14,000	0	1,068,422

Future Development	1		253,312	76,472			148,164				477,948
	1A					261,360	261,360				522,720
	2			149,154	298,309		149,154		4,000	96,000	696,617
	3			235,224	470,448		235,224				940,896
	4			228,690	457,380		228,690				914,760
	5				98,450	107,399	134,169	150,000		120,000	610,018
Subtotal		0	253,312	689,540	1,324,587	368,759	1,156,761	150,000	4,000	216,000	4,162,959

Total Anticipated Development	1	2,789,053	253,312	76,472			148,164				3,267,001
	1A					261,360	261,360				522,720
	2		110,400	759,886	298,309		247,293		4,000	96,000	1,515,888
	3			235,224	470,448		235,224				940,896
	4			228,690	457,380		228,690				914,760
	5				98,450	107,399	715,551	159,600	14,000	120,000	1,215,000
Subtotal		2,789,053	363,712	1,300,272	1,324,587	368,759	1,836,282	159,600	18,000	216,000	8,376,265

Note: Activity square footage may vary and may transfer between Planning Areas provided the use intensity is consistent with the E.I.R. "Trip Generation Budget" and allowed within the Planning Areas.

Exhibit 7



4.1 LAND USE PLAN

The McDonnell Centre Business Park Specific Plan divides the project site into six Planning Areas (1,1A,2,3,4,&5). The purpose of identifying separate Planning Areas is to create a distinct cluster of activities and allow for individual project development to occur in a timely manner, within an overall Master Plan Concept. This approach recognizes development phasing patterns, market conditions and establishes sufficient flexibility to provide for the opportunity of a variety of activities within each Planning Area.

AREA 1

Planning Area 1 is the core of the project area and includes the existing McDonnell Douglas Aerospace Facility. The current facility is composed of 2,789,053 square feet of building area on 100 net acres of land. It is anticipated that the aerospace facility will continue to modernize and expand in the coming years. Anticipated short-term improvements include outside storage facilities and additions to some of the of existing buildings. Future projects may include aerospace related manufacturing activities, such as tank fabrication and assembly, heavy welding, insulation and thermal protections, additional office uses can also be accommodated.

Additional new industrial development and new activities or reuse plan or restructuring of the existing aerospace operations and facility can be accommodated by the regulations of the Specific Plan. This area can accommodate an additional 477,948 square feet of new development, for a total development of 3,267,001 square feet.

AREA 1A

Planning Area 1A is the entrance to the Aerospace Facility and provides the primary parking facilities along Bolsa Avenue. The area consist of 20 net acres and is anticipated to be developed as additional McDonnell Douglas research and development operations. Area 1A may also provide new opportunities for private office development, research and development facilities and/or commercial retail activities. This area can accommodate a total development of 522,720 square feet.

AREA 2

Planning Area 2 includes the area along Springdale Street and Bolsa Avenue, westerly to Able Lane; and comprises 58 net acres of land. This Planning Area will provide opportunities for new industrial related development projects. Sharp Electronics is currently constructing a 538,859 square foot facility on a 23 acre site, and Cambro Manufacturing recently completed a 120,000 square foot building on a 12 acre site, a second and third phase will add 160,412 square feet with an ultimate development of 280,412 square feet.

The remaining acreage in the Planning Area, is currently vacant and is expected to be developed with research and development facilities, light industrial, warehouse and/or distribution uses. Office uses and some commercial and entertainment activities may also occur. This area can accommodate a total development of 1,515,888 square feet.



LAND USE SUMMARY

Planning Area	Industrial Use		Office Use		Commercial Use		TOTAL	
	(sq. ft.)	(ac)	(sq. ft.)	(ac)	(sq. ft.)	(ac)	(sq. ft.)	(ac)
1	3,118,837	96	148,164	4	-		3,267,001	100
1A	-	-	522,720	20	-		522,720	20
2	1,168,595	46.5	247,293	8	100,000	3.5	1,515,880	58
3	705,672	28	235,224	8	-		940,896	36
4	686,070	27	228,690	8	-		914,760	35
5	98,450	4	822,950	21	293,600	15	1,215,000	40
TOTAL	5,777,624	201.5	2,205,041	69	393,600	18.5	8,376,265	289*

Note: * indicates net acres. There are 307 gross acres. 18 acres in right-of-way

Exhibit 8

AREA 3

Planning Area 3, west of Area 2, includes the Bolsa Avenue frontage west of Able Lane to an extension of Graham Street. The area is predominately vacant and consists of 36 net acres. The area is intended to accommodate research and design facilities along with light industrial, manufacturing and distribution uses. Office use and limited commercial retail activities along the Bolsa Avenue frontage may occur. Anticipated development patterns in Area 2 and 3 will be very similar and compatible. This area can accommodate a total development of 940,896 square feet.

AREA 4

Planning Area 4 is a ribbon of land on the northern perimeter of the project area. The area consists of 35 net acres of vacant land intended to be developed as an expansion of the current aerospace facility (Planning Area 1). An alternative is for all or a portion of the Planning Area 4 to be separated from the aerospace facility and develop as new research and development facilities, office development, manufacturing, warehousing and/or distribution uses. This area can accommodate a total development of 914,760 square feet.



AREA 5

Planning Area 5 is the western boundary of the project area, with a significant amount of frontage on the arterial highways. The area consists of 40 net acres and partially developed with office, research and development facilities and surface parking lots. The phase one, of a two phase office development has been completed and consists of an eight story, 235,831 square foot office building with adjacent parking (constructed in 1989); phase two proposes a twelve story, 345,551 square foot office building, with an adjacent 14,000 square foot restaurant and 9,600 square foot of support commercial services.

Future activities for the area will depend on market conditions and may include: office, light industrial, research and development, and retail commercial uses including hotels and restaurants. These development activities may be either an expansion of the aerospace facility or independent new projects. This area can accommodate a total development of 1,215,000 square feet.

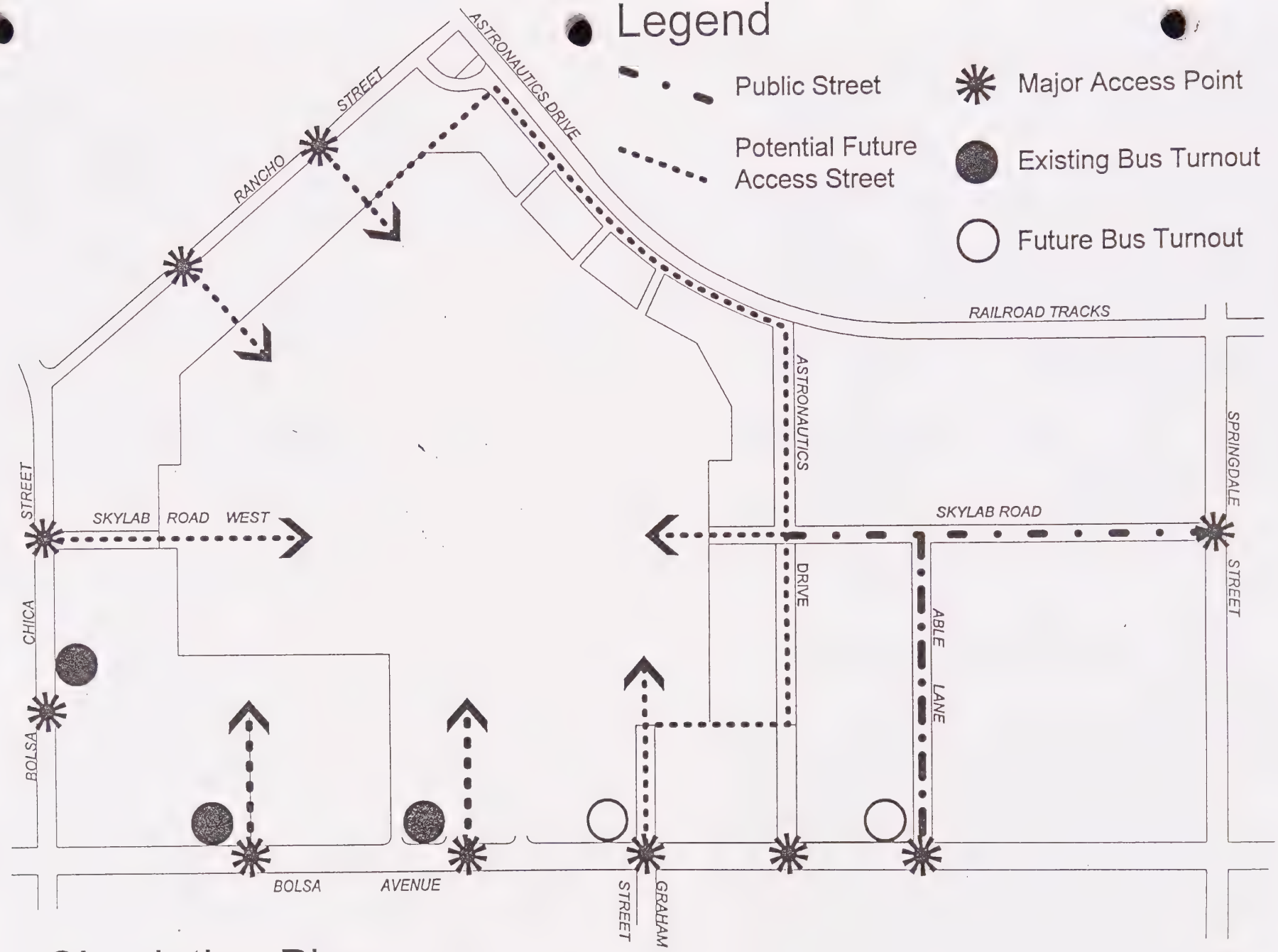
4.1.0 POLICIES:

- 4.1.1 The existing McDonnell Douglas Aerospace Facility may expand from Planning Area 1 into Planning Areas 1A, 3, 4, and 5; concurrent with a Site Plan Review and Parcel Map application.
- 4.1.2 Industrial related uses, particularly light manufacturing, assembly and research and development, shall be the primary intended activity within the project area and shall be permitted in all Planning Areas.
- 4.1.3 Commercial retail uses shall be limited to activities which cater to industrial uses (i.e. restaurants, hotels, etc.) and shall be permitted only in Planning Areas 1A, 2, 3, and 5.
- 4.1.4 Industrial related office uses shall be allowed in all Planning Areas with greater concentration anticipated in Planning Areas 1A, 3, and 5.
- 4.1.5 Development adjacent to residential uses must be sensitive to the concerns of the residents.



Legend

- Public Street
- Potential Future Access Street
- Major Access Point
- Existing Bus Turnout
- Future Bus Turnout



Circulation Plan

Exhibit 9



4.2 CIRCULATION PLAN

The Circulation Plan illustrates the general alignments, classifications, location and design of cross-sections for public and private streets within the area of the Specific Plan. The Circulation Plan is consistent with the Huntington Beach General Plan's Circulation Element.

Access to the City of Huntington Beach and the McDonnell Centre Business Park is provided by two regional highways; Interstate 405 and State Route 22 (San Diego and Garden Grove Freeways). The City's General Plan designates the intersection of Bolsa Chica Street and Rancho Road as a primary entry node to the City. Access to the project site is provided by a system of arterial highways including:

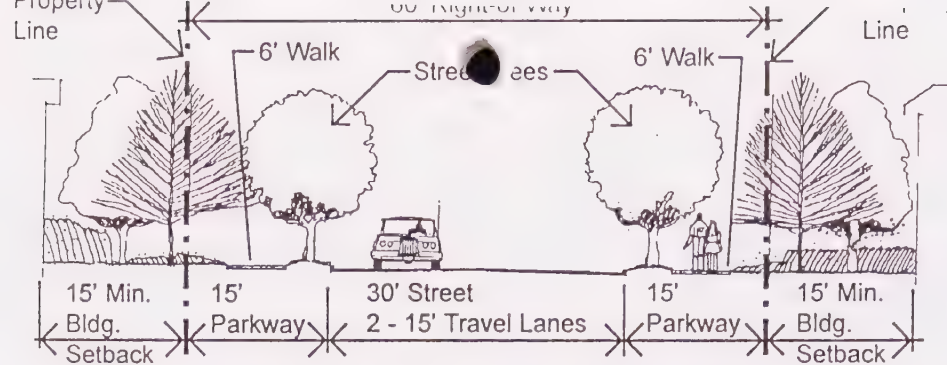
- Bolsa Chica Street, a north-south major arterial highway (120 foot right-of-way); designated as a truck route and a primary path/image corridor.
- Springdale Street, a north-south primary arterial highway (100 foot right-of-way); designated as a truck route and a secondary path/image corridor.
- Bolsa Avenue, an east-west major arterial highway (120 foot right-of-way); designated as a truck route, and a primary path/image corridor and landscape corridor.
- Graham Street, a north-south secondary arterial highway (80 foot right-of-way); designated as a truck route.

- Rancho Road, a north-south secondary arterial highway (80 foot right-of-way).

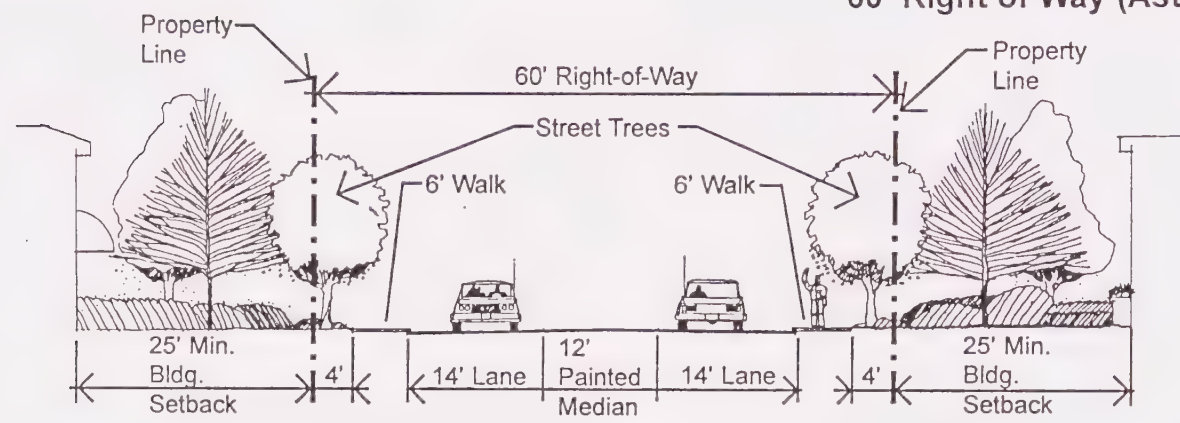
Internal circulation is currently provided by a network of public and private streets serving as access to individual parcels within the project area. Circulation is further enhanced by a number of entry drives and public transportation facilities. The interior streets within the Specific Plan are: Able Lane, Astronautics Drive, Graham Street, Skylab Road, and Skylab Road West. Able Lane, Skylab Road and Skylab Road West are designed for eighty (80) foot right-of-ways; all other interior streets are designed with sixty (60) foot right-of-ways. The intersection of Skylab Road and Astronautics Drive is all-way stop controlled. All other streets stop at their respective intersection with Skylab Road or Astronautics Drive. There are currently signalized intersections at Graham Street and Bolsa Avenue, Able Lane and Bolsa Avenue and Skylab Road and Springdale Street; and McDonnell Drive and Bolsa Avenue. In addition a limited number of private driveways provide access to individual parcels from the surrounding arterial highways.

The Circulation Plan is designed to accommodate a number of different development scenarios. The overall circulation concept relies on a hierarchy of circulation features ranging from major arterials to local streets. The system is designed to accommodate traffic to the project area and around the area while discouraging through traffic intrusion into individual Planning Areas.

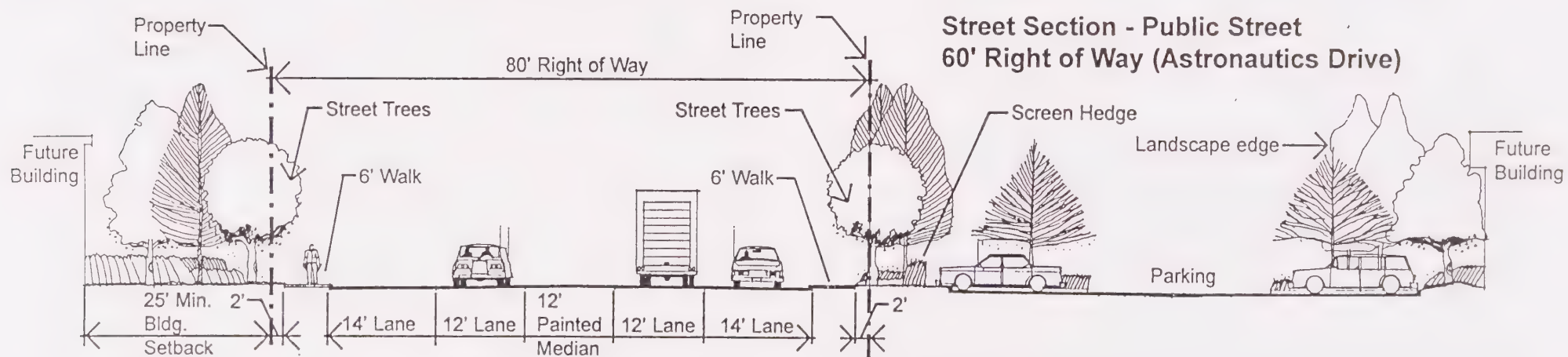




Street Section - Private Street
60' Right of Way (Astronautics Drive)



Street Section - Public Street
60' Right of Way (Astronautics Drive)



Street Section - Public Street
80' Right of Way (Skylab Road and Able Lane)

Street Cross Sections

Exhibit 10



The Circulation Plan provides for a phasing of street improvements to correspond to the phased development in each Planning Area. All streets shown on the Circulation Plan are public streets unless otherwise indicated.

Astronautics Drive will function as a private road for access to the McDonnell Douglas Aerospace Facility. New development in Planning Area 4, other than an expansion of the aerospace facility, may trigger the need to further improve Astronautics Drive. New development in Planning Area 1A, including expansion of the aerospace facility, may also bring about the need to extend the on-site public street system in order to facilitate the creation of additional parcels. This concept is reflected in the Circulation Plan. This additional public street would be designed similar to the other on-site streets with one travel lane in each direction and sixty (60) feet of right-of-way.

Skylab Road West will remain a private access street, however, it will be improved as an eighty (80) foot right-of-way. The need to extend Skylab Road as a public street through the entire project area would only occur if a major reconfiguration of the present aerospace facility is proposed. This type of major land use change may require a number of circulation alternatives and layouts, however, such a change is not anticipated.

In order to efficiently facilitate new development parcels, the primary access will be from interior streets. Direct access from adjacent arterials will be subject to review and approval of the Director of Public Works. Primary access locations into the project area have been located

and designed to provide full turning movements (Exhibit 9). The locations relate to existing driveways and median designs, and are anticipated to adequately serve the projected traffic volumes for the project area. Specific future development proposals may require modifications to these anticipated access locations.

The circulation system shall be master planned to accommodate the buildout of the Specific Plan area. Initial street construction and future phased reconstruction shall be completed in advance of occupancy of new facility phased construction. The Director of Community Development and the Director of Public Works shall approve phasing plans for street improvement construction, consistent with development construction phasing and implemented through the Parcel Map process.

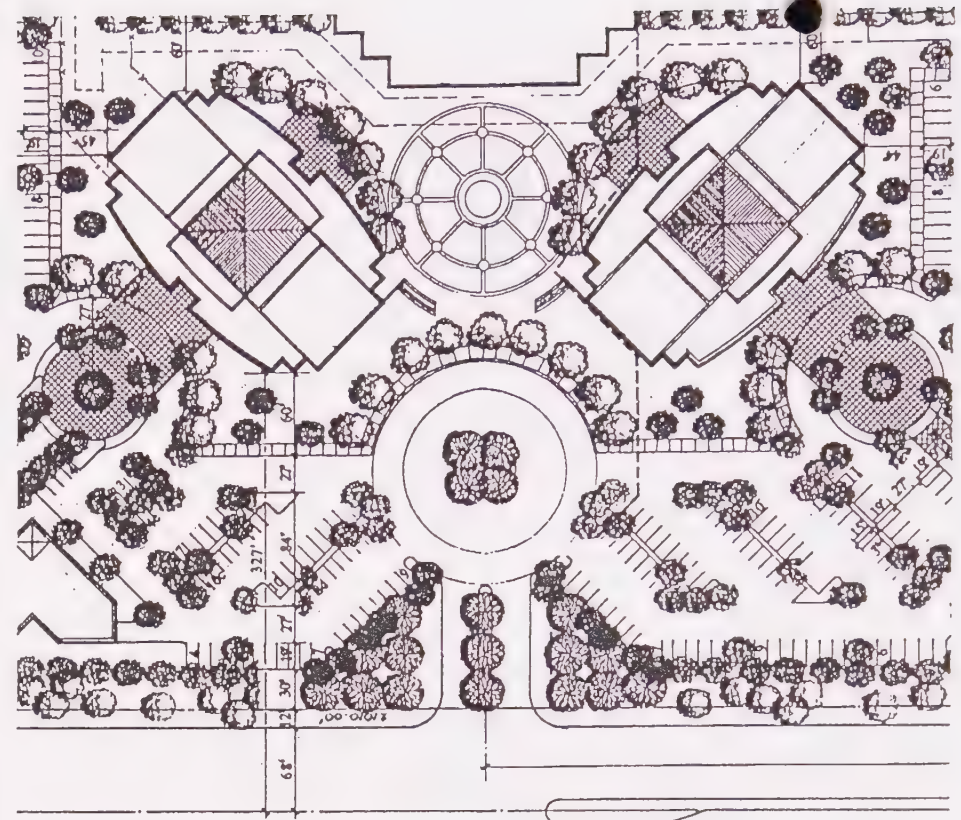
Alternative forms of transportation should also receive careful consideration. The current OCTA bus route passes the project area on Bolsa Avenue. The project Circulation Plan identifies existing and proposed bus turnout locations along Bolsa Chica Street and Bolsa Avenue. As a supplement to vehicular access to the project area, potential future access may be available from the rail line on the northern boundary of the site. The Circulation Plan preserves access to this line for future options.

In addition, the Master Plan Concept encourages the preservation and expansion of the pedestrian walkway system. As a means of achieving a strong landscape image, pedestrian walkways are required and shall be provided in the street right-of-way adjacent to new development projects.



4.2.0 POLICIES:

- 4.2.1 **Skylab Road** shall be designed as a major east/west street. Skylab Road shall be improved and dedicated as a public street with an eighty (80) foot right-of-way; from Springdale Street west to the eastern entry of the aerospace facility.
- 4.2.2 **Able Lane** shall be designed as the major north/south street. Able Lane shall be improved and dedicated as a public street with an eighty (80) foot right-of-way from Skylab Road to Bolsa Avenue.
- 4.2.3 **Skylab Road and Able Lane** right-of-ways shall include a sixty-four (64) foot curb to curb (allowing for two travel lanes in each direction and left turn lanes) with sufficient area for a six (6) foot sidewalk and two (2) foot landscape area on both sides of the street.
- 4.2.4 **Primary access to the project area** shall be from the existing signalized intersections of Skylab Road and Springdale Street, Able Lane and Bolsa Avenue, Graham Street and Bolsa Avenue, and McDonnell Drive and Bolsa Avenue. Other access locations into the project area shall occur only where traffic patterns and median openings allow, subject to review and approval of the Director of Public Works and Fire Chief.
- 4.2.5 **Direct access to individual projects** from the arterial highways adjacent to the project area shall be limited and allowed only when the project, size, location or type of use, warrants such access, subject to review and approval of the Director of Public Works and Fire Chief (Exhibit 11).



Private Drive Entry
Exhibit 11

- 4.2.6 **Deceleration and acceleration lanes** for individual developments may be required, depending on the location of the proposed access point. Right turn in and right turn out accesses to the arterial highways shall be considered on an individual project basis, subject to the review and approval of the Director of Public Works.
- 4.2.7 **Shared access** facilities and reciprocal vehicular access to and between individual projects may be requested and/or required by the Director for adjacent uses and parcels.

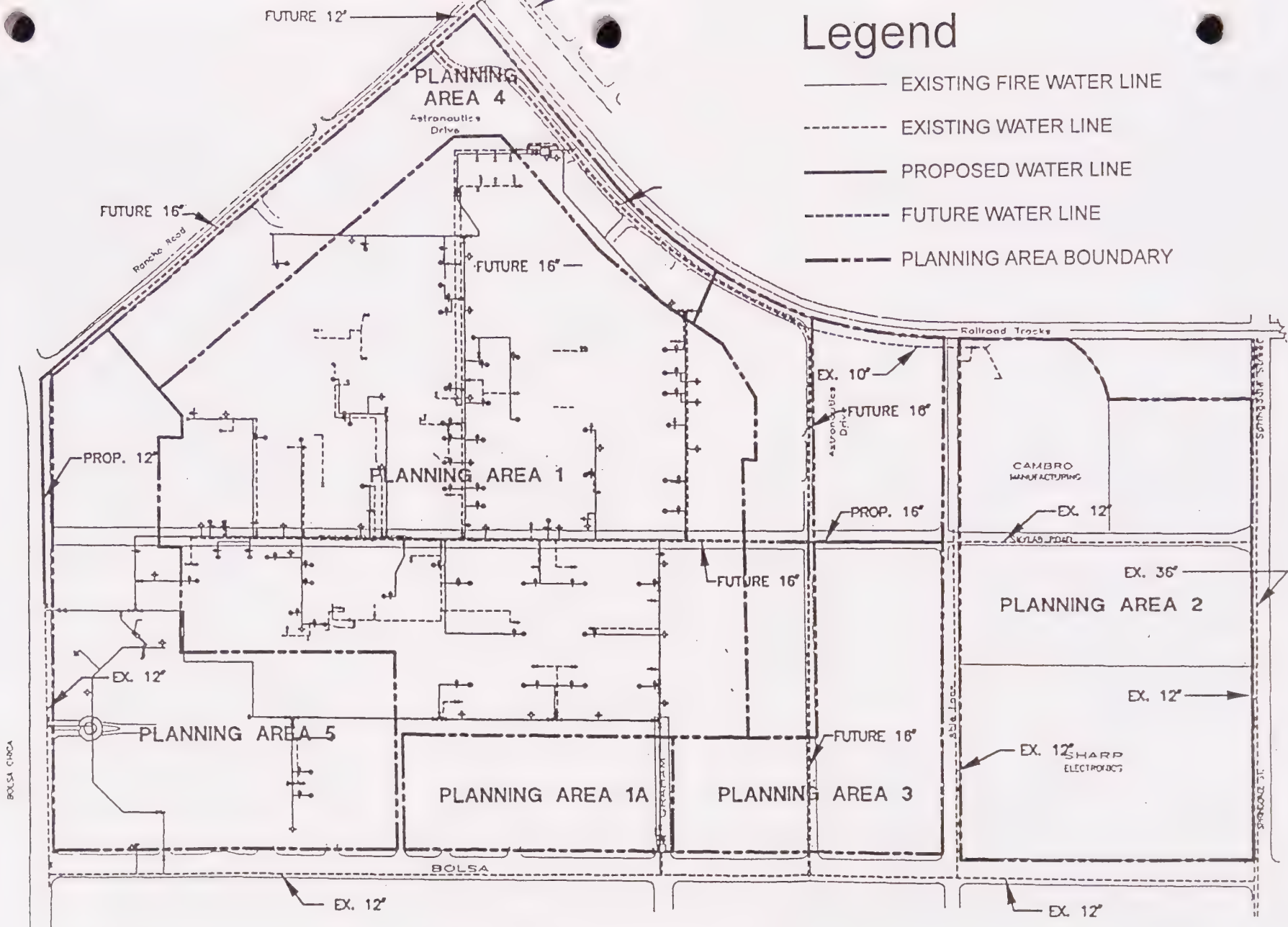


- 4.2.8 **Astronautics Drive** will continue to serve as a private street and shall be designed to accommodate a public right-of-way of sixty (60) feet, between Rancho Road and the southern terminus. Street dedication will only be required if and when warranted by traffic demand, as determined by the Director of Public Works.
- 4.2.9 **Alternative transportation** forms of including OCTA Bus System and future rail access, shall be investigated with each development project.
- 4.2.10 **Pedestrian sidewalks** shall be incorporated into each development project as a component of an individual projects landscape plan. Sidewalks shall be installed on both sides of the street adjacent to new development throughout the project area.
- 4.2.11 **Public landscape areas** within the right-of-ways will require a separate Parkway Landscape Agreement from the adjacent property owner, for continued maintenance of the area.
- 4.2.12 **Public and private streets** shall be developed to local street standards as shown in the Standard Plans of the City's Public Works Department.
- 4.2.13 **On-street parking** shall not be permitted anywhere in the project area, on both and public streets.
- 4.2.14 **Additional traffic impact analysis** may be required, due to unanticipated project developments subject to review and approval by the Directors of Community Development and Public Works.
- 4.2.15 **The "Trip Generation Budget"** identified in the E.I.R. shall be reviewed with each individual project request. Additional periodic reviews, by the Director of Public Works, will also be necessary for overall compliance with the E.I.R.
- 4.2.16 **Circulation system improvements** shall be master planned to accommodate ultimate buildout of the Specific Plan. On-site and off-site circulation improvements shall be completed prior to occupancy of any adjacent new development.



Legend

- EXISTING FIRE WATER LINE
- - - EXISTING WATER LINE
- PROPOSED WATER LINE
- - - FUTURE WATER LINE
- - - PLANNING AREA BOUNDARY



Water System Plan

Exhibit 12



4.3 PUBLIC FACILITIES PLANS

The Public Facilities Plans identify existing and proposed infrastructure, storm drain, sewer and water facility improvements to serve development within the Specific Plan area. A specific analysis of infrastructure requirements and detailed design, construction and phasing plans can be found in the Infrastructure Master Plan Technical Appendix to the E.I.R. and bound under a separate cover.

4.3.0 POLICY:

Developers within the Specific Plan area will be responsible for the construction of public facilities improvements concurrent with individual project development, subject to review and approval of the Director of Public Works.

4.3.1 Water System Plan

Domestic water for the property will be provided by the Public Works Water Division of the City of Huntington Beach. The Water Division provides water to all of the customers within the City of Huntington Beach.

The Water Division has use of both underground and imported water sources to service the area. The underground supply comes from nine existing wells, and imported water delivered to the City of Huntington Beach by the Metropolitan Water District (MWD) at three locations. The Specific Plan area is part of the City's Master Plan for Water Service and the ultimate development anticipated will be adequately served by the City's systems.

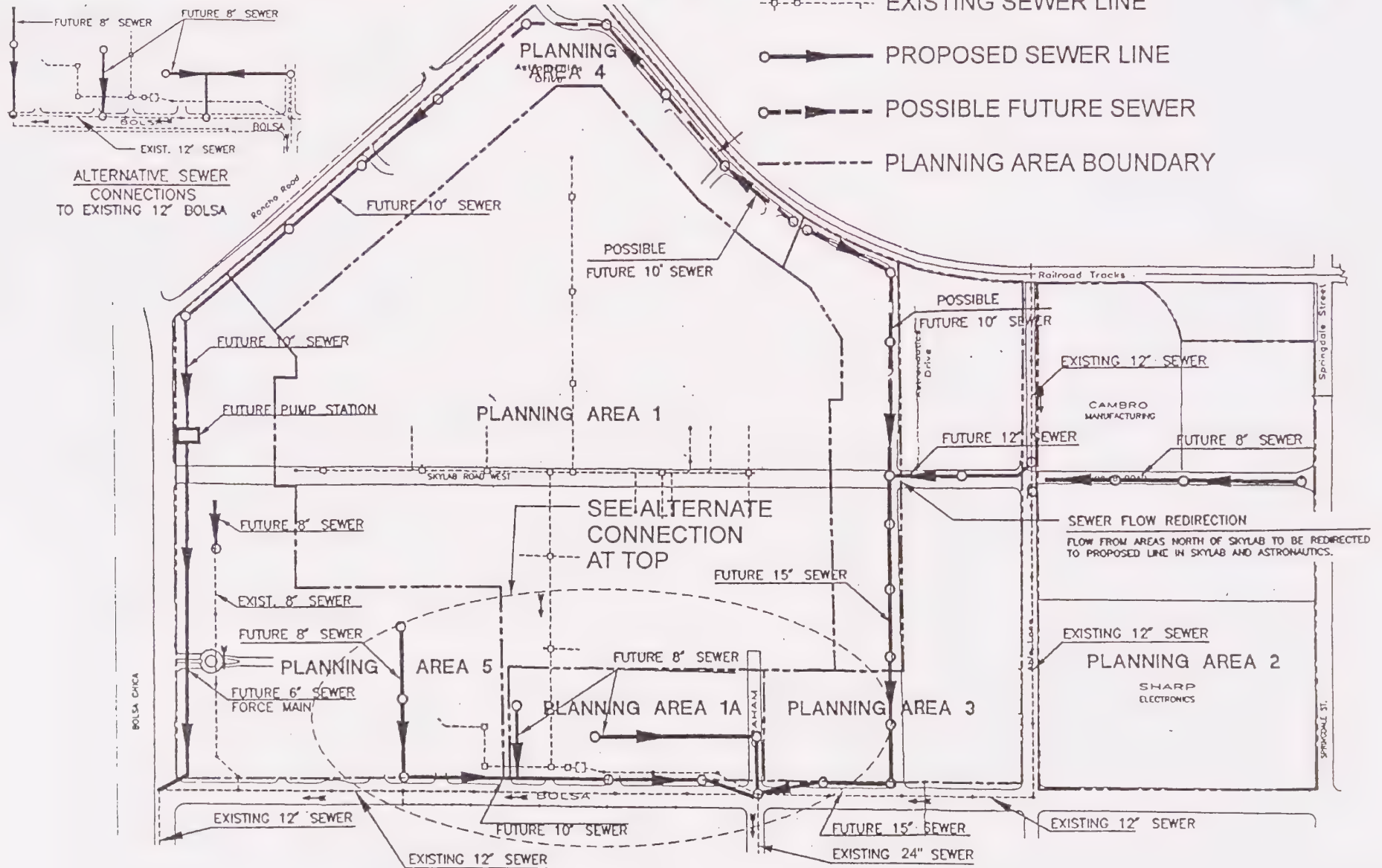
MWD is the major wholesale water purveyor to the City of Huntington Beach which, in turn, is the retail provider to all water users in the City, including the subject property. The existing and proposed water supply systems are shown on the Water System Plan (Exhibit 12). On-site water lines can connect to the external system at more than one location to provide a second point of service (or loop) to each part of the system. All the on-site lines will be sized to deliver fire flow at adequate quantities and pressures and are eight (8) to sixteen (16) inches in diameter.

All water improvements will be designed to the City of Huntington Beach water standards for future City acceptance and maintenance. The existing McDonnell Douglas Aerospace (MDA) water systems facility will be connected, through meters, to the proposed additions to the City's water system, but will remain independent of the City's system. Locations of fire hydrants and apparatuses will be reviewed for each individual development by the Fire Department and Water Division of the City of Huntington Beach to ensure adequate fire flow and pressure.



Legend

- EXISTING SEWER LINE
- PROPOSED SEWER LINE
- POSSIBLE FUTURE SEWER
- PLANNING AREA BOUNDARY



Sewer System Plan

Exhibit 13



North

4.3.2 Sewer System Plan

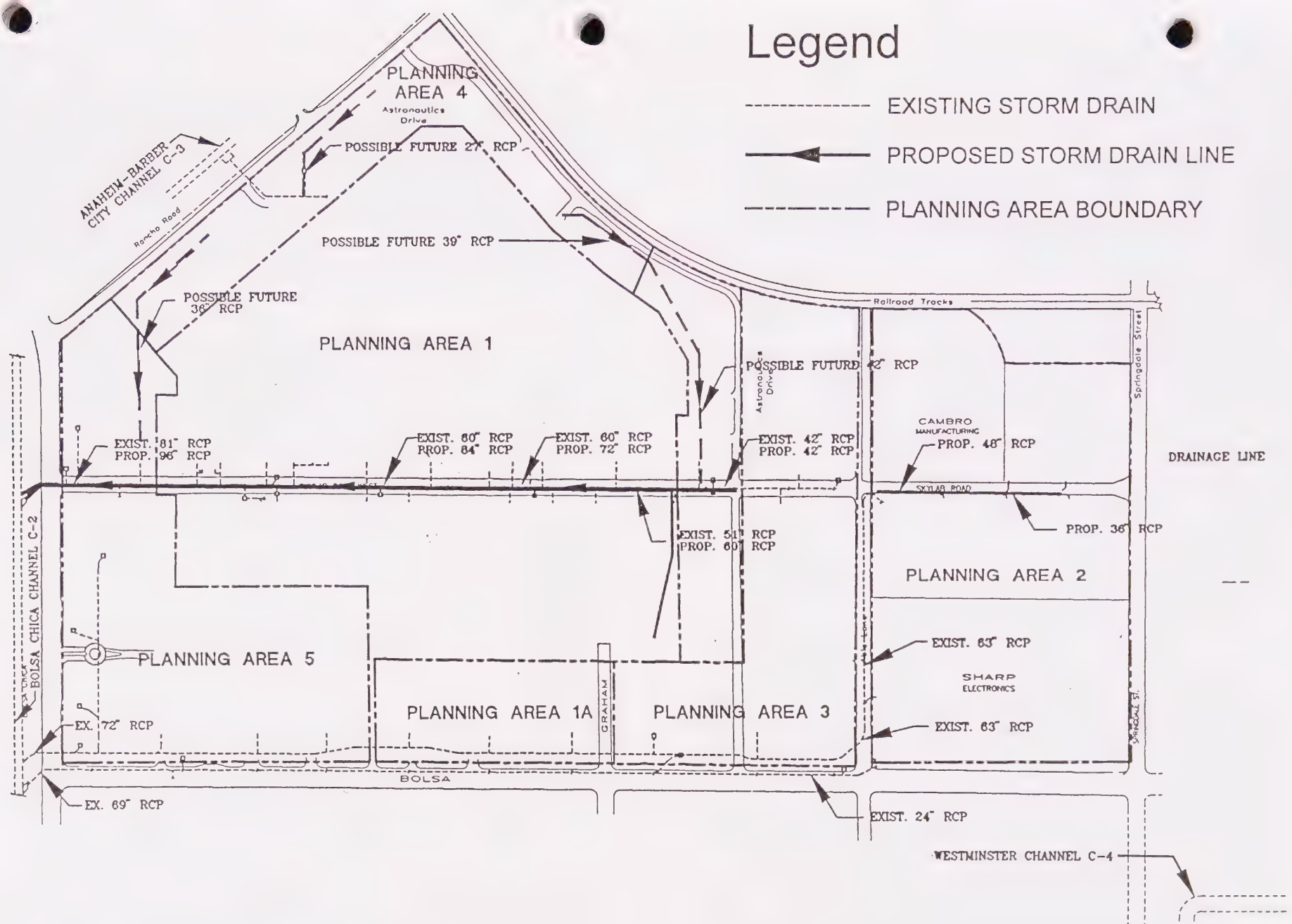
The City of Huntington Beach is responsible for the review and approval of the collection of wastewater within the project area, and the County Sanitation Districts of Orange County (CSDOC) are responsible for the treatment of wastewater. The Sewer System Plan (Exhibit 13) depicts the existing sewer system which serves the project area. The existing system is comprised of several lines, including twelve (12) inch lines in Able Lane; an eight (8) inch line located in Skylab Road east of Able Lane and a fifteen (15) inch line located in Skylab Road west of Able Lane to Astronautics. These existing systems connect to the City sewer in Bolsa Avenue.

The City system ultimately is collected by the Sanitation District via their trunk and distribution lines to convey sewage to their plant. District Plant #5 is located in Fountain Valley, and District Plant #2 is easterly of the City approximately twelve miles from the project area. Sewer service for the property is collected at two locations. One is at the intersection of Bolsa Avenue and Graham Road, then via a twenty-four (24) inch line southerly to the Sanitation District trunk line in Edinger Avenue and continuing to the District plant #2. This system also collects the sewerage flows from the residential area northerly of the property. The second collection point is at the intersection of Bolsa Avenue and Bolsa Chica Street then via a twelve (12) inch line southerly to the Sanitation District's trunk line in Edinger Avenue.

The existing McDonnell Douglas Aerospace (MDA) facility private sewer system will not be connected to the proposed peripheral sewer systems, and will remain isolated from this new system, and will remain so as long as the existing aerospace facility remains. The existing MDA sewer system has sufficient capacity as a stand alone system.

Sewer lines within the property will be contained in public or private roads or in easements that will ultimately be dedicated to the City of Huntington Beach. Due to the existing flat, natural grade within the project area, pipe sizes will be in the range of eight (8) inch minimum to fifteen (15) inch maximum with the need to incorporate a private lift station and six (6) inch force main at Bolsa Chica/Skylab. Also, the sewer system will be designed to the City of Huntington Beach sewer standards for possible future public acceptance and maintenance. The proposed sanitary sewer system is shown on the Sewer System Plan (Exhibit 13).





Storm Drainage Plan

Exhibit 14



4.3.3 Storm Drainage Plan

The City of Huntington Beach and the Orange County Flood Control District are the agencies responsible for the flood control system in the project vicinity. Regional flood control channels exist along Bolsa Chica Street and Springdale Street. The Bolsa Chica channel, an open channel, is located adjacent to the western boundary of Bolsa Chica Street. The existing storm drain system provides drainage for the site, draining the majority of the site to the west, towards Bolsa Chica channel. A small eastern portion of the site drains to the channel adjacent to the eastern boundary of Springdale Street.

There are three existing storm drain systems surrounding the project area: The area to the east draining southerly into the Orange County Flood Control District. (O.C.F.C.D.) C-4 Westminster Channel; and the area to the south draining westerly into the O.C.F.C.D. C-2 Bolsa Chica Channel; the areas to the west and to the north drain to the O.C.F.C.D. C-2 Bolsa Chica Channel and to the C-3 Anaheim Barber City Channel, respectively. Through the approximate center of the property, drainage is piped westerly to the O.C.F.C.D. C-2 Bolsa Chica Channel. This piped system is at its maximum capacity, serving the existing McDonnell Douglas aerospace facilities.

In the event that the MDA facility would no longer remain, and the area became available for new development, the Master Plan Drainage Study proposes to provide a new piped drainage facility paralleling the existing (or replacing the existing entirely), draining to the C-2 Bolsa Chica Channel. The areas proposed at the project's northerly boundary will drain northerly into the O.C.F.C.D. C-3

Anaheim Barber City Channel. The project's most easterly area and southerly areas are currently tabled to drain into the newly constructed storm drain system adjacent to Bolsa Avenue, as approved by the O.C.F.C.D. and the City of Huntington Beach Master Plan of Drainage programs for these proposed systems. No detention basins, outside of Planning Area 1, will be necessary to serve the existing and proposed ultimate development. Only the peripheral edges, which can be accommodated now by the existing downstream facilities and piped systems in place, are being considered at this time. The residential drainage areas northerly of the project area have their own area drainage facilities, and do not affect the proposed property.

The storm drain systems are considered to be Reinforced Concrete Pipe (RCP) with the minimum pipe size of eighteen (18) inches. The pipe sizes are estimated for planning purposes only and are subject to refinement in the final design of the project. The storm drains lie within existing private streets or within easements to allow for maintenance of the completed system. All drainage is on-site and only the termination of the tributary facilities, as they propose to enter into the O.C.F.C.D. channels, will require any jurisdiction permission for public right-of-way construction. The existing downstream facilities will have negligible flow impacts based upon the proposed ultimate development drainage calculations. The entire drainage system for the project is shown on the Storm Drainage Plan (Exhibit 14). The ultimate location and sizes of the proposed storm drain lines may vary from that shown on the plan.



4.3.4 Water Quality

Water quality in California is regulated by the U.S. Environmental Protection Agency's National Pollution Discharge Elimination System (NPDES), which controls the discharge of pollutants to water bodies from point and non-point sources. A NPDES permit or other E.P.A. review will be required for individual construction projects.

Prior to issuance of any grading permit, the developer shall submit a "Notice of Intent" (NOI), along with the required fee to the State Water Resources Control Board to be covered under the State NPDES General Construction permit and provide the City with a copy of the written reply containing the discharger's identification number. The NPDES permit process does require that a permit application contain a project drainage report, along with the submittal of the Storm Water Pollution Prevention Plan (SWPPP) worksheet. Both the report and the worksheet identify any watercourses affected by construction activity, and a comprehensive listing of drainage Best Management Practice (BMP) mitigations that must be provided.

Through the NPDES Permit process, the City currently requires contributors to non-point runoff pollution to establish Best Management Practices (BMP's) to minimize the potential for pollution. Under this program, the developer is responsible for identification and implementation of a program of BMP's which can include special scheduling of project activities, prohibitions of certain practices, establishment of certain maintenance procedures, and other management practices to prevent or

reduce the pollution of downstream waters. Typical elements of such a BMP program would include addressing the use of oil and grease traps, detention basins, vegetation filter strips, and other common techniques in order to preclude discharge of pollutants to local storm drains and channels.

4.3.5 Utilities

There are several public utility service providers in the McDonnell Centre Business Park area.

Although adequate facilities exist for the current service needs of the area, additional facilities may be required as additional development occurs.

4.3.6 Electricity

Electrical service to the area is provided by the Southern California Edison Company. Existing transmission and distribution lines are adequate to service current and potential future needs. Individual development projects may be required to relocate or underground existing facilities concurrent with other improvements and consistent with the City's Undergrounding Ordinance (17.64). An exception to this provision is the 66Kv line adjacent to Able Lane.



4.3.7 Natural Gas

Natural gas service in the Specific Plan area is provided by the Southern California Gas Company. Adequate facilities exist for current and projected future needs. Developers may be required to relocate existing facilities concurrent with project development.

4.3.8 Telephone

Telephone service in the Specific Plan area is provided by General Telephone (GTE). Developers should coordinate with GTE for the relocation of existing facilities and installation of new service.

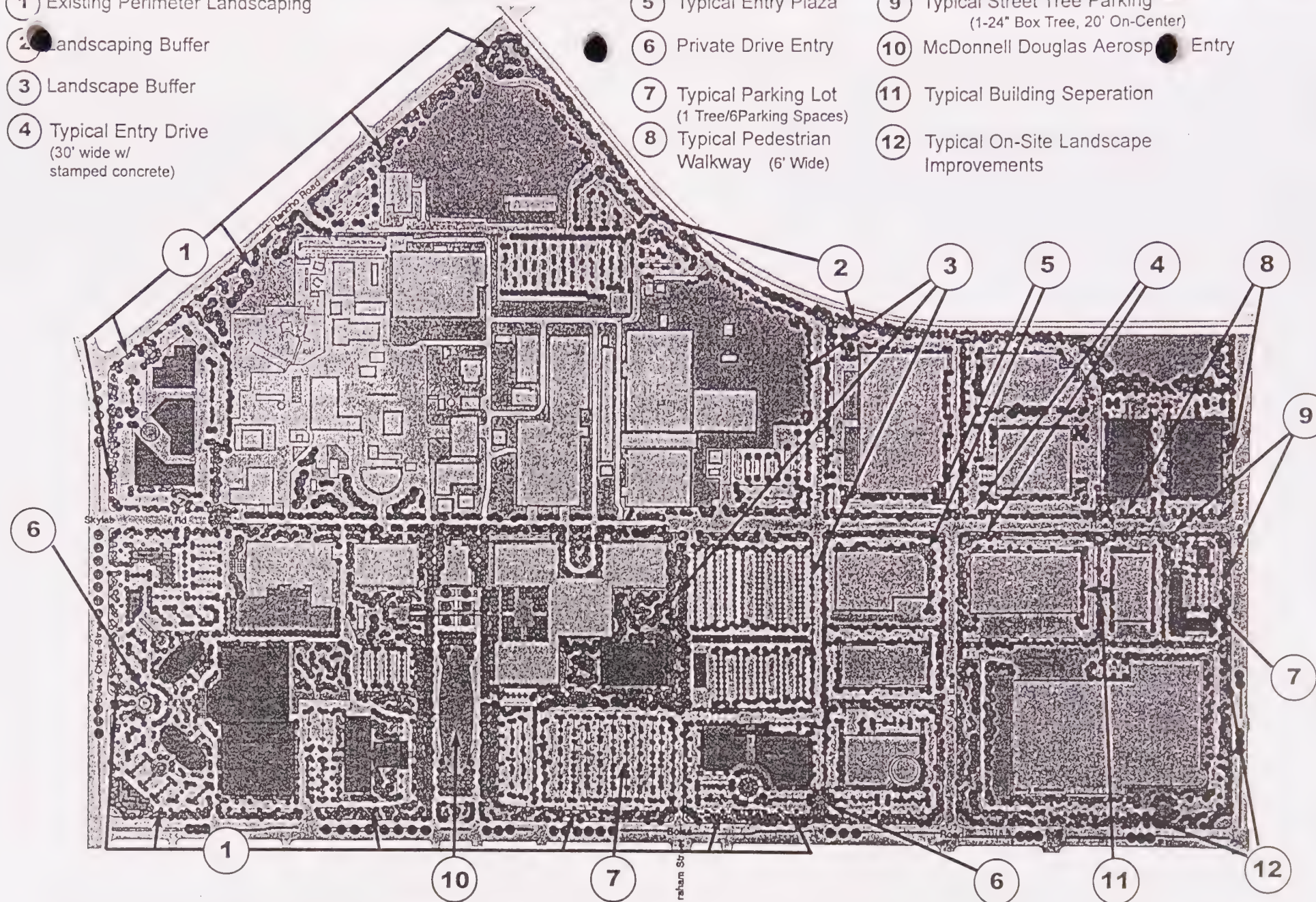
4.3.9 Cable Television

Cable television service within Huntington Beach is provided by Time Warner Communications. Developers should coordinate with the Cable Company for the installation of new service.

4.3.10 Solid Waste Disposal

Rainbow Disposal Company currently provides solid waste disposal services for the area. Based on service projections and anticipated demand increase, an adequate level of service will be maintained. No solid waste disposal facilities are planned to be located in the Specific Plan area.





Landscape Concept Plan

Exhibit 15



Note: This illustrative shows a hypothetical development scenario on the project site.



4.4 LANDSCAPE CONCEPT

The Landscape Concept for the McDonnell Centre Business Park is an integral component of the overall project design. The Landscape Concept is intended to establish the design character and visual qualities of the interior and perimeter of the project area. The overall landscape theme is composed of several design elements, including: the public arterials, local and private streets, entryways, access drives, parkway areas, transitional edges and security fencing and walls which create a cohesive project area image.

To establish a consistent planting streetscape, all streets shall be established with identifiable landscape materials, drawn from the Plant Palette. The Plant Materials Palette (Exhibit 38) includes turf, shrubs, ground cover and trees which are compatible with the City's overall landscaping requirements and consistent with the existing adjacent streetscape.

The landscape plan for each parcel shall be designed to reflect the overall Landscape Concept. Individual project landscaping shall be designed to conform with the Specific Plan's Landscape Guidelines (Section 5.3). Planning and installation of each project's landscaping shall be subject to review and approval by the Director of Public Works. Off-site improvements for each development shall include a landscape area with a six (6) foot sidewalk. Pedestrian walkways shall be required on both sides of all public and private streets as a necessary unifying component to the landscape theme.

Entryways to the project area and individual developments must be carefully designed. Each private drive

way shall be consistent with one another in size, appearance, and signage. Major entryway locations into the project area will allow for the business park entry monument signs within the perimeter landscaping.

Project area walls screening and fencing along the perimeter arterials shall be compatible for each project and provide project identity, privacy and noise control. Individual wall treatments shall reflect the architectural character of the adjacent main buildings and be compatible with other buildings throughout the project area. Walls along individual property lines shall be designed to provide security and privacy while providing visual compatibility; see-through wrought iron fencing may be effectively incorporated as a security element.

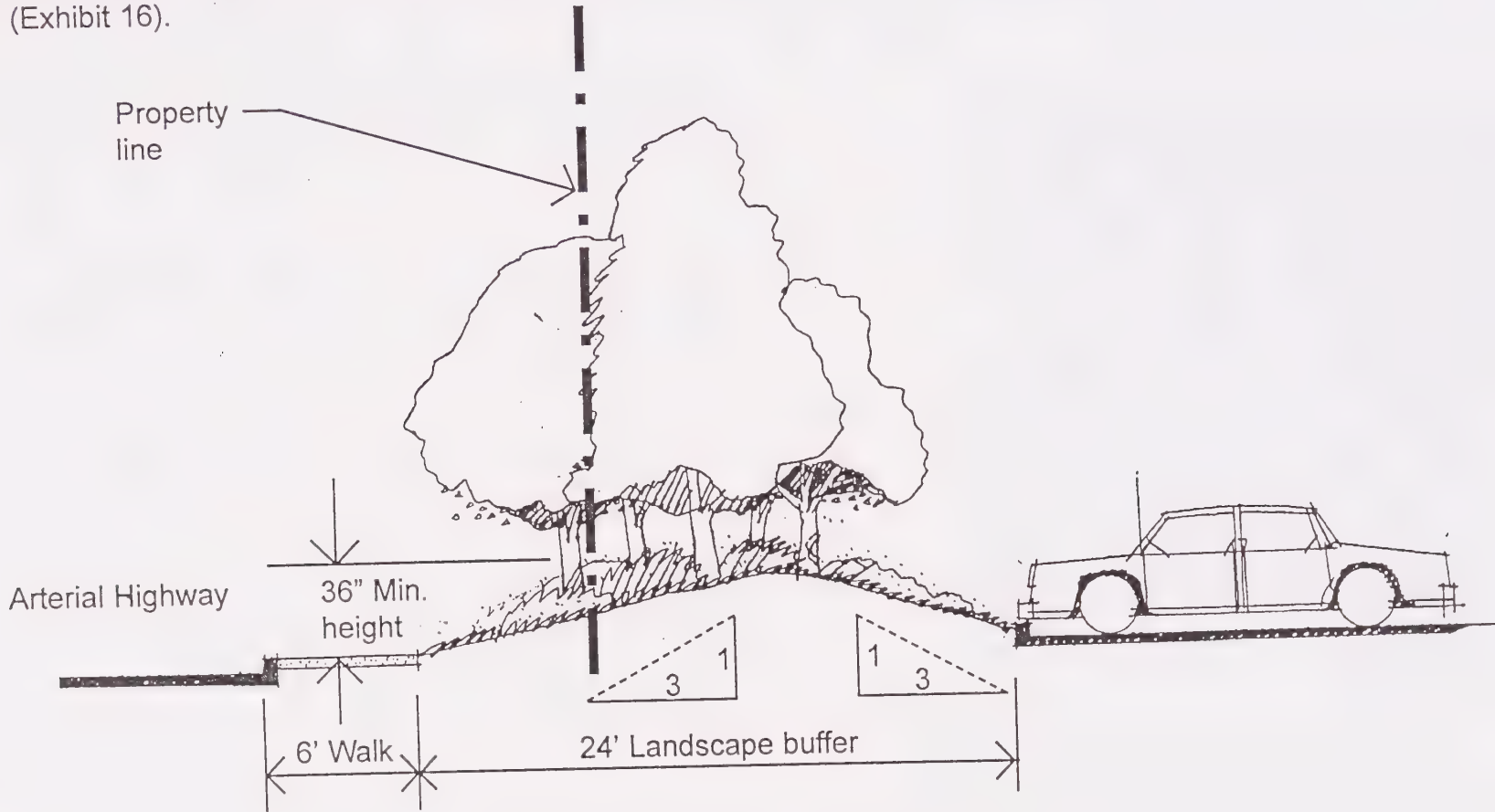
The Landscape Concept establishes the primary unifying design element for the project area. The streetscape design is intended to preserve and enhance the existing layout and variety of landscape patterns. This includes the incorporation of landscaped areas adjacent to the perimeter arterials, landscaped pedestrian walkways within the right-of-way of interior streets, the preservation of existing tree lines, where feasible, and creating a consistency of design for private drives, access points and parking lot layouts.

The Landscape Concept is intended to create an overall theme with specific guidelines. However, alternative approaches may be considered which preserve the intent of the guidelines while proposing modifications to the regulations. All individual landscape plans shall comply with the following policies.

4.4.0 POLICIES:

4.4.1 **Perimeter landscaping** shall preserve or construct, a minimum twenty-four (24) foot wide landscape buffer between the arterial highway and private project improvements, including buildings, walls, parking areas, etc (Exhibit 16).

4.4.2 **Landscape medians**, located in the arterial highways adjacent to the project area, shall be designed and constructed per City of Huntington Beach standards and approvals, and shall be maintained by the City.



Landscaping on Perimeter Streets

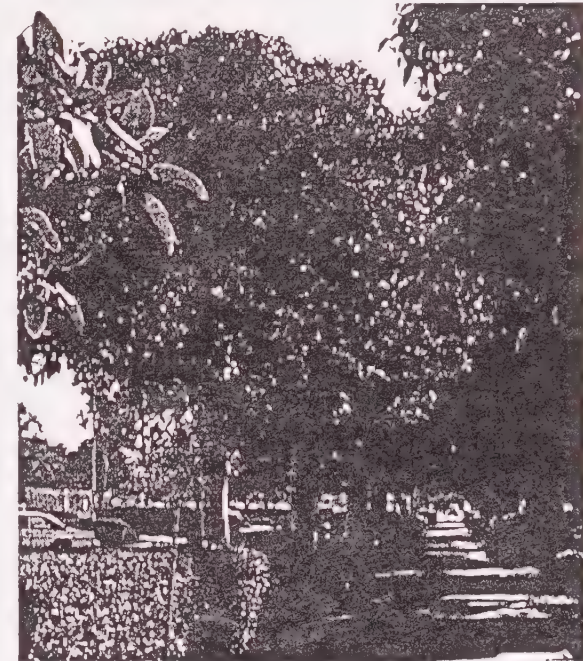
Landscape Detail

Exhibit 16

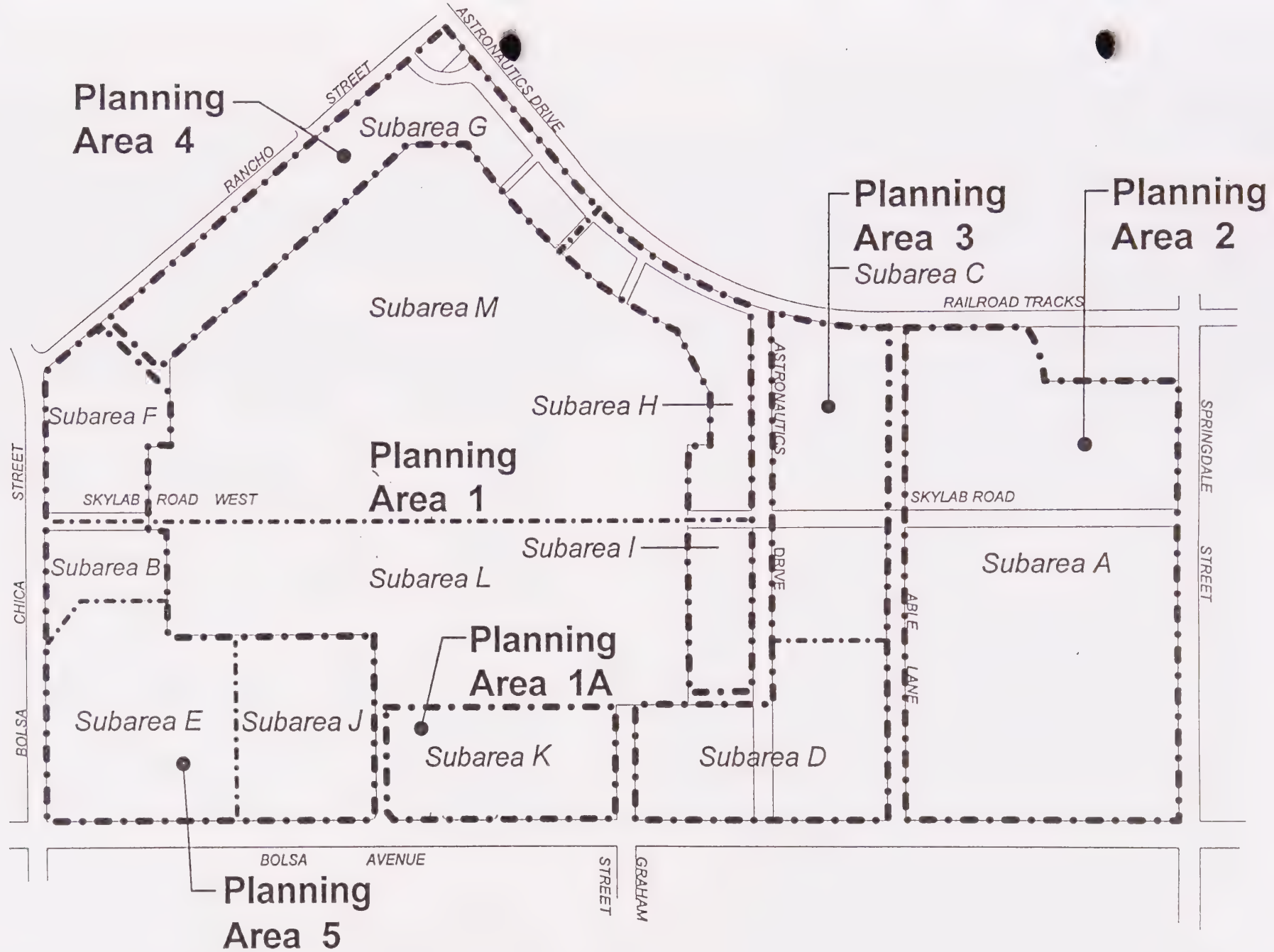


- 4.4.3 **Private landscape improvements** and adjacent right-of way landscape improvements shall be constructed by individual project developers and maintained by each adjacent/abutting property owner, consistent with the overall landscape theme.
- 4.4.4 **Private landscape improvements** shall provide sufficient landscaping to implement the Landscape Concept and shall incorporate a minimum of ten (10) percent of the net project site.
- 4.4.5 **Landscape buffer areas** shall be provided in transitional edges between abutting Planning Areas, if not separated by a public or private street. A minimum buffer area shall be fifty (50) feet in width and shall include landscaping and berming to adequately screen adjacent uses.
- 4.4.6 **Landscaped buffer areas** shall be maintained by the private development in accord with the project area landscape theme.
- 4.4.7 **Private project entryways** shall be designed with enhanced landscaping area, textured pavement and be able to accommodate entry monumentation and lighting.
- 4.4.8 **Slope areas** shall be permanently landscaped with water efficient plant materials, and erosion control methods acceptable to the Director of Public Works.
- 4.4.9 **Irrigation systems** shall comply with the City's "Water Efficient Landscape Requirements." (Ordinance #1452).

- 4.4.10 **Existing healthy trees** (as determined by a consulting arborist) removed as part of landscape improvements shall be replaced on a two for one basis with thirty-six (36) inch box trees or an approved equivalent, as determined by the Director of Public Works.
- 4.4.11 **All landscaping** shall conform with the requirements of Chapter 232 (Landscape Improvements) of the Huntington Beach Zoning and Subdivision Ordinance, the City Arboricultural and Landscape Standards and Specifications and City Standard Plans, in addition to the Specific Plan policies in Section 5.3.0.



Pedestrian Walkway
Exhibit 17



Phasing Plan

Exhibit 18



4.5 PHASING PLAN

The McDonnell Centre Business Park will be developed in various phases over the next several years. In order to accommodate the anticipated intermittent development patterns, all required circulation, infrastructure and community improvements within each Planning Area of the Specific Plan shall be completed prior to, or simultaneously with, individual projects.

The Specific Plan Planning Areas (1, 1A, 2, 3, 4, & 5) have been further divided into Subareas (A thru M) to reflect the anticipated development pattern and infrastructure improvement phasing. The Phasing Plan presents a schedule of project development based on an incremental installation of infrastructure improvements.

The Phasing Plan recognizes that the project area is presently almost forty (40) percent built-out, including the McDonnell Douglas Aerospace Facilities, with an additional ten (10) percent under construction and/or entitled. The first phase for new projects will occur in the eastern portion of the Specific Plan area. The western portion, along Bolsa Chica Street, will develop in later phases, as the market conditions warrant, however there is the potential for a hotel project at Bolsa Chica Street and Skylab Road West. The infrastructure phasing schedule has been prepared to provide assurance that adequate public facilities and services will be available for each new project.

The first phase of the infrastructure improvements will extend, install and upgrade the utilities necessary to provide for new projects in Planning Areas 2 and 3. Total first phase of infrastructure improvements will be completed by the year 1998.

In later phases infrastructure improvements will be extended west along the southern boundary of the project area. This extension of services will facilitate a variety of new development options in Planning Areas 1A and 5. Later phases of development will follow the market trends and build out accordingly.

Planning Area 4 will most likely be the last area to develop. The most likely scenario for this Planning Area is to remain part of the aerospace facility. The area is well suited and situated for expansion of the present aerospace facility, and will provide for an opportunity to replace lost parking, as new development projects occur on the existing aerospace parking lots. The utilities necessary for expansion of the aerospace facilities may be sufficient with the existing services, depending on the nature of the projects. Independent new development of Planning Area 4 will trigger the need for the extension of utility lines from the first phase improvements in Areas 2 and 3.

The maximum permitted floor area for all industrial, office and commercial developments within the Specific Plan Area shall not exceed the General Plan floor area ratio (F.A.R.) of .75. The cumulative square footage of floor area for each of the allowed uses within the Specific Plan have been estimated for each Planning Area; however, these numbers may be modified and transferred as market conditions and/or development concepts dictate. Any modifications and/or adjustment to land use square footage ratios must comply with the "Trip Generation Budget" and other provisions identified in the E.I.R. and City's General Plan.



Infrastructure Improvements

Planning Area	2	5	3	3	5	5	4	4	4	5	1A	1	1
Sub Area	A	B	C	D	E	F	G	H	I	J	K	L	M
Sewer	●					●	●	●				●	●
Storm Drain	●					●	●					●	●
Water	●	●				●	●					●	●
Street paving	●	●	●	●	●	●	●	●	●	●	●	●	●
Lighting	●	●	●	●	●	●	●	●	●	●	●	●	●
Asphalt Overlay	●	●		●	●	●	●	●	●	●	●	●	●
Traffic Signal	●					●							

Note: To be implemented through the Parcel Map process.

No additional infrastructure improvements will be required for a continuation of the existing aerospace activities.

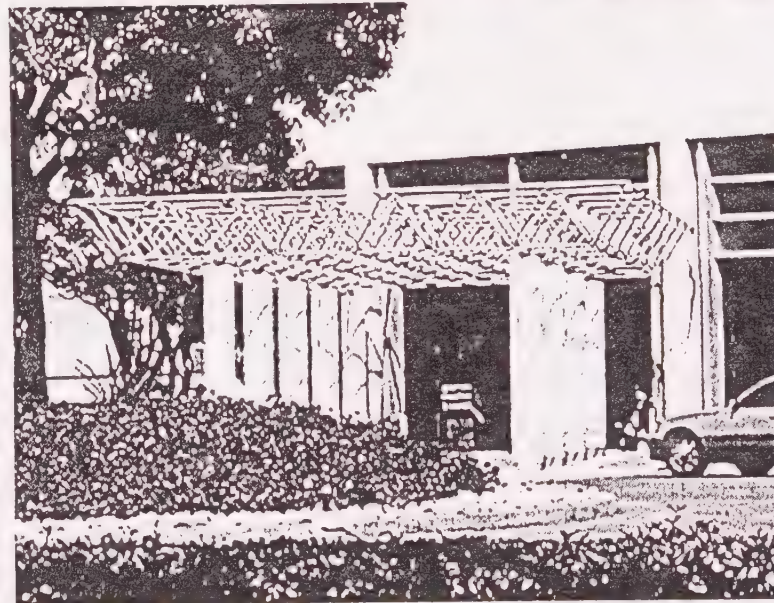
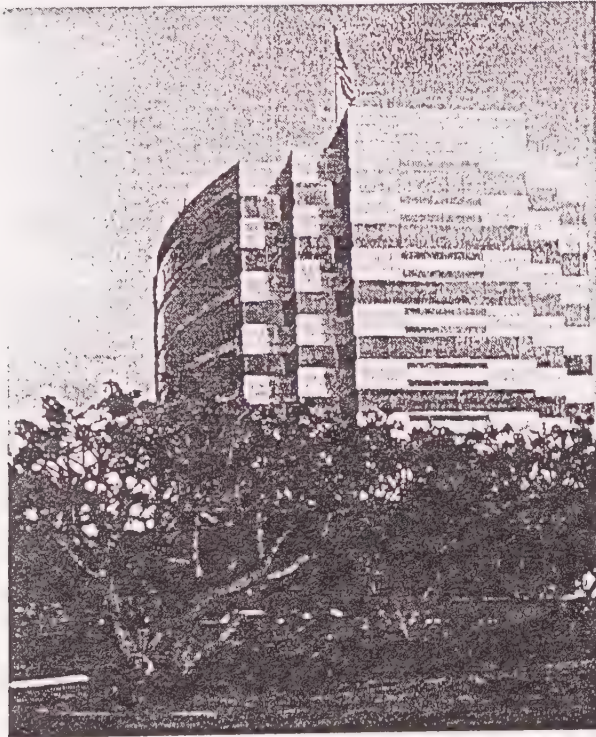
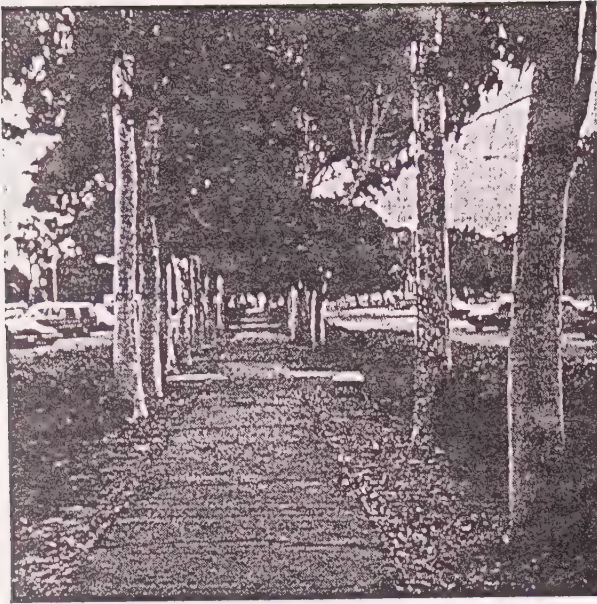
Schedule of Improvements

Planning Area	2	5	3	3	5	5	4	4	4	5	1A	1	1
Sub Area	A	B	C	D	E	F	G	H	I	J	K	L	M
1998	100%	100%											
2000			100%	100%	25%			50%	50%				
2005						50%			50%	25%			
2010					25%	50%		25%		25%	50%		
2015					25%			25%		25%	50%	100%	
2020					25%		100%			25%			100%

Exhibit 19

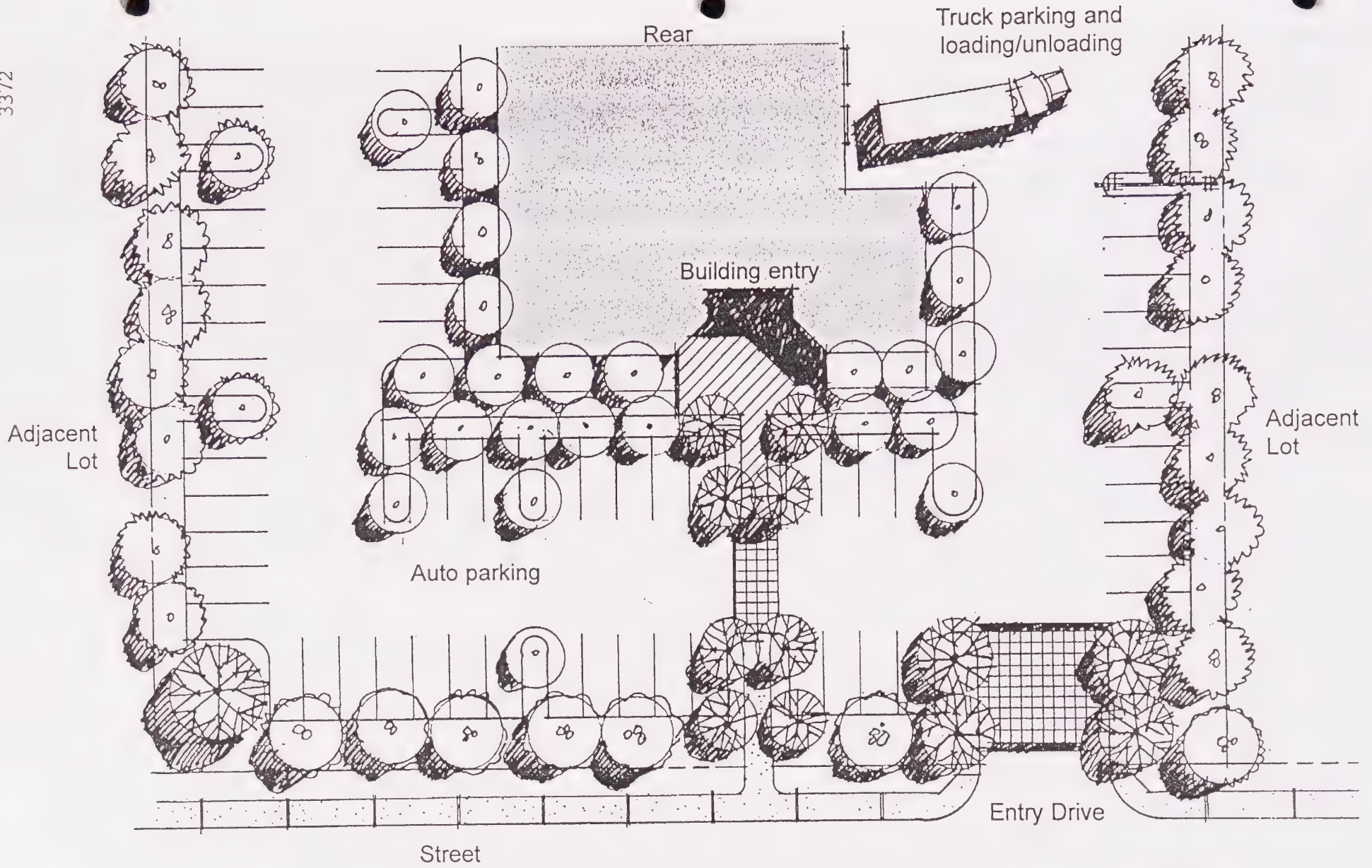


DESIGN GUIDELINES



Section Five





Typical Site Layout

Site Planning Detail

Exhibit 20



DESIGN GUIDELINES

5.0 PROJECT AREA CHARACTER

The Design Guidelines establish the character and style for the development of a business park complex with buildings and streetscapes that have a distinctive visual identity. The Guidelines accommodate individual development identities and promote interrelationships between complementary land uses and community features. The major elements of the Design Guidelines include: site planning, architecture, streetscape, landscaping, and signage. All development proposals within the Specific Plan area shall conform to the Design Guidelines and shall incorporate appropriate theme elements.

The Design Guidelines are to be used by the private developer and the City of Huntington Beach as part of the Site Plan Review process. The Design Guidelines are general and may be interpreted with some flexibility in their application to specific projects. Variations may be considered for projects with special design characteristics that still meet the objectives of the Guidelines. The Design Guidelines shall be used to promote a high level of design quality while at the same time provide some flexibility, necessary to encourage creativity on the part of individual project designers.

The Design Guidelines have been prepared to articulate the intended development standards of the Specific Plan area. The Guidelines establish a framework for developers/designers of individual projects; and design criteria, which the City will use to evaluate proposed developments.

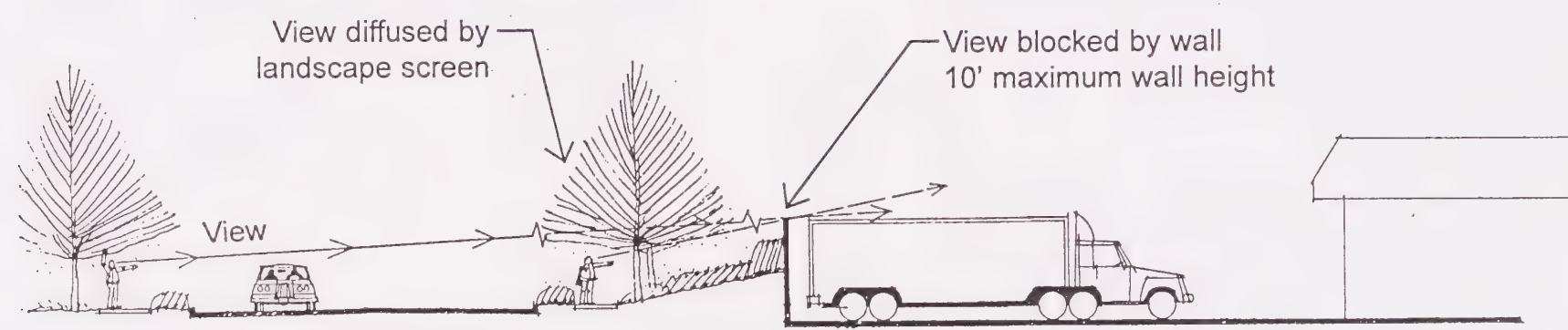
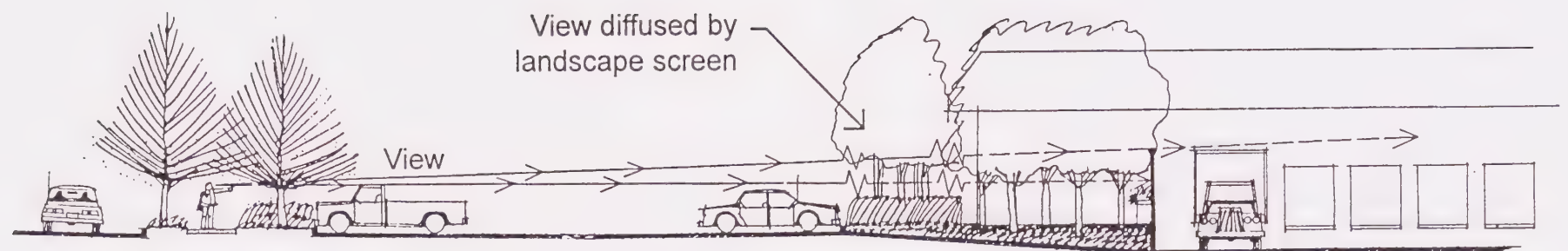
5.1 SITE PLANNING GUIDELINES

The Specific Plan anticipates a combination of industrial, office, and commercial buildings, each varying in parcel size, building height, and intensity of development. The Design Guidelines section provides the measure by which basic concepts for coordinated site planning can be realized. Care must be taken in the creation of each parcel in the project area to provide convenient access, and cluster common activities. Effective site planning techniques will establish a strong outline and framework for guiding future individual development projects, and create a unique high quality business park.

The successful integration of effective site planning techniques, with the basic design elements on individual projects, will enhance the visual experience in the Specific Plan area, and promote a true sense of place. The existing McDonnell Douglas facilities along Bolsa Avenue and Bolsa Chica Street and the development of Cambro Manufacturing and Sharp Electronics on the eastern boundary have established a high quality of design.

Future development of the remaining parcels will be in accordance with many of the established initial design concepts, in order to achieve an overall project area compatibility.





Screening of Loading and Storage Areas

Site Planning Detail

Exhibit 21



To facilitate the development of the McDonnell Centre Business Park into a unique resource for the community, the following site planning policies shall be complied with.

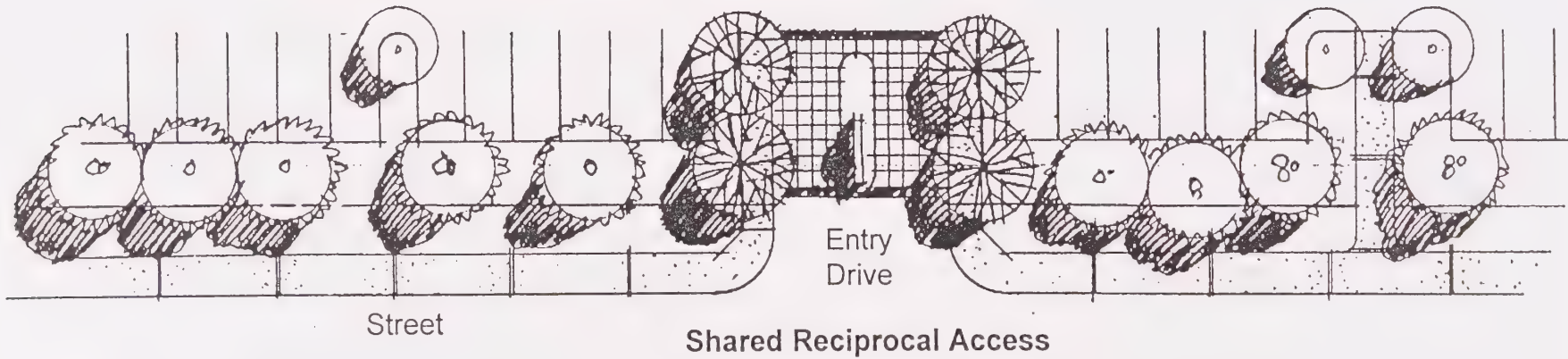
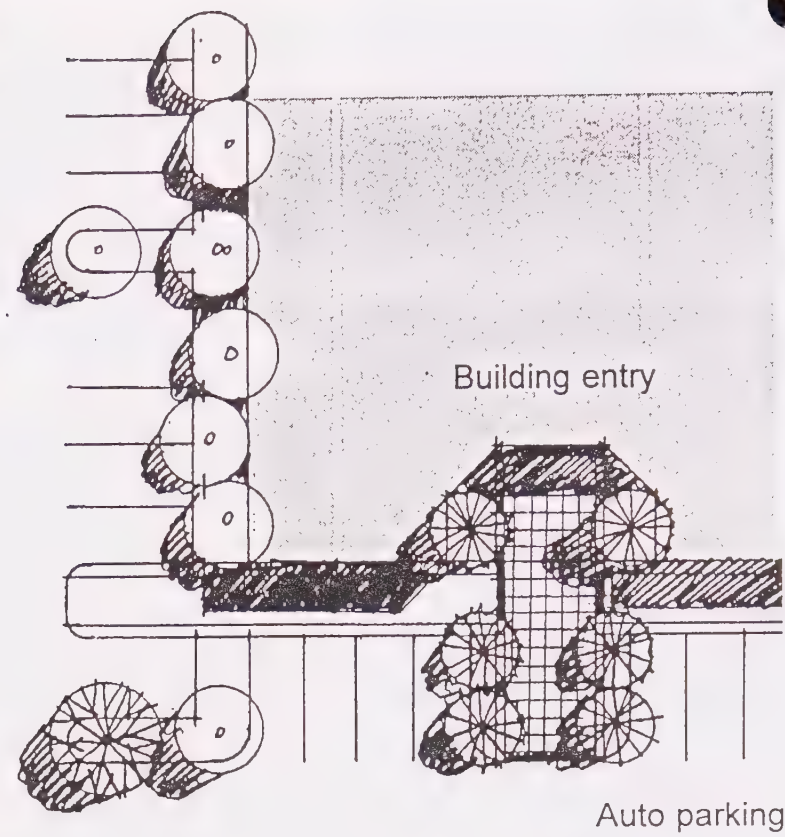
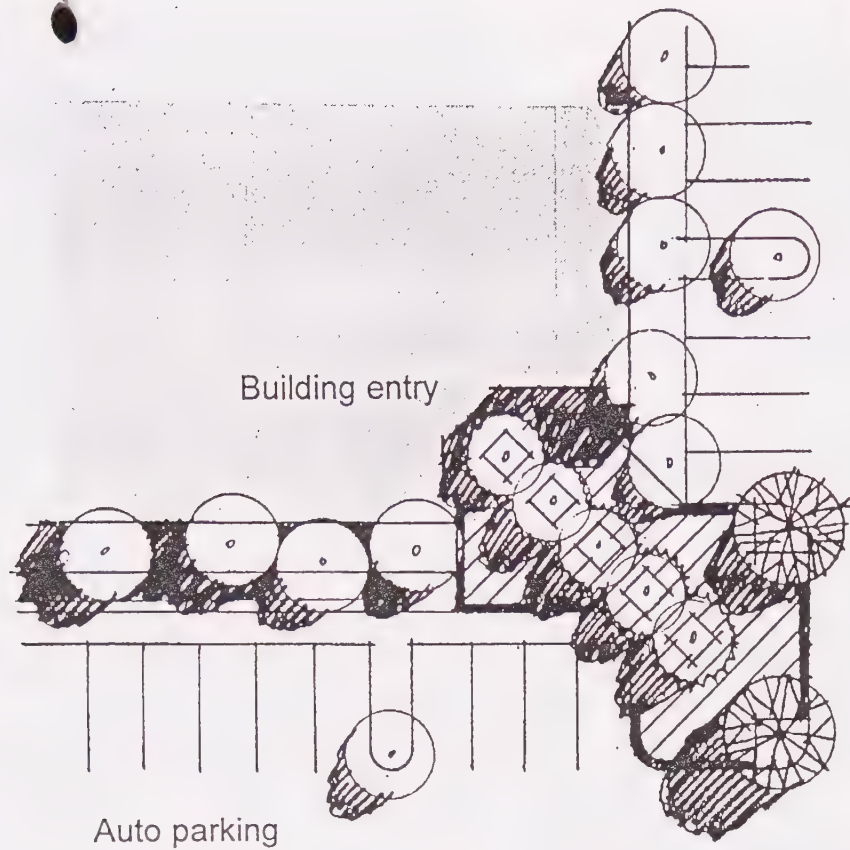
5.1.0 POLICIES:

- 5.1.1. **Site layout** for individual projects shall be designed to route people and vehicles through the site in a clear, identifiable, efficient and effective manner (Exhibit 20).
- 5.1.2 **Building orientation** and access shall be designed compatible with previously constructed and/or approved projects. Building entryways, administration areas and other window areas shall front on to any adjacent street (Exhibit 22).
- 5.1.3 **Parking** for individual projects shall be provided on site in a manner that is convenient and compatible with the layout and design of the overall project area. In order to create larger building setbacks surface parking facilities shall be located between the main building front facade and any adjacent street. A buildings side facade may abut a street subject to the setback provisions in the Development Regulations section (Section Six).

- 5.1.4 **Loading and storage areas** including truck access shall be in the rear and /or side portions of the lot, and screened from the street. All truck maneuvers (i.e. backing into truck loading areas) shall be performed within the project site (Exhibit 21).

- 5.1.5 **Site access** to the individual projects shall provide the minimum required turning radius (inside radius of seventeen (17) feet and outside radius of forty-five (45) feet) and roadway widths for all drive aisles and fire lanes (twenty-four (24) feet), consistent with the adopted City standard. Additional fire and emergency considerations shall be addressed on a project by project basis, subject to review and approval of the Fire Chief (Exhibit 23).

- 5.1.6 **On-site circulation and access** should be compatible and enhance adjacent developments. Shared driveways and entry locations may be proposed. Driveways and entrances on opposite sides of a street shall align. Any proposed offset of driveways on opposite sides of a street shall be subject to review and approval of the Director of Public Works (Exhibit 22).

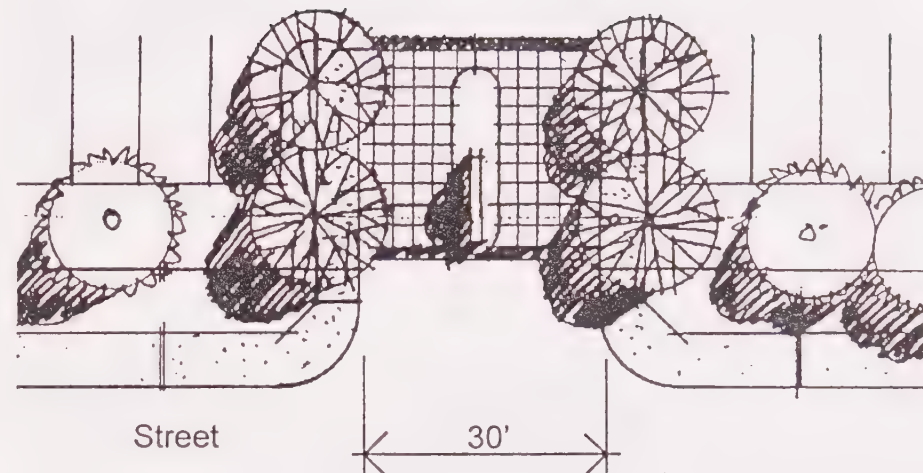


Site Planning Detail

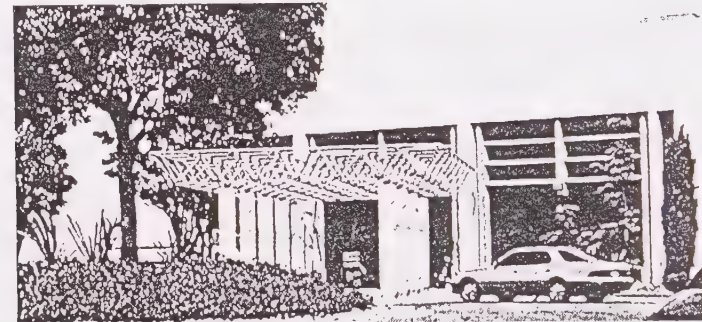
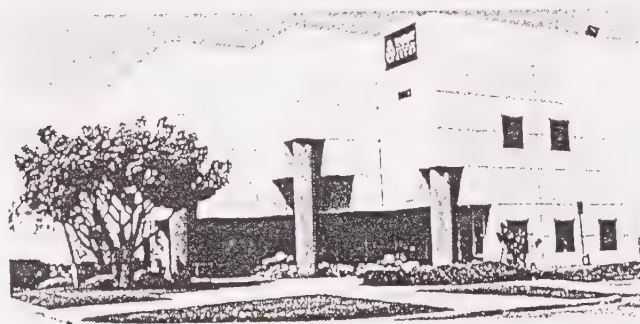
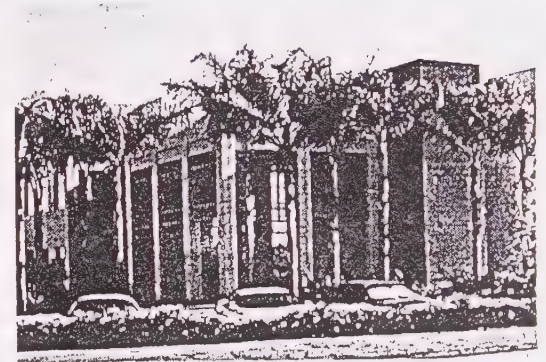
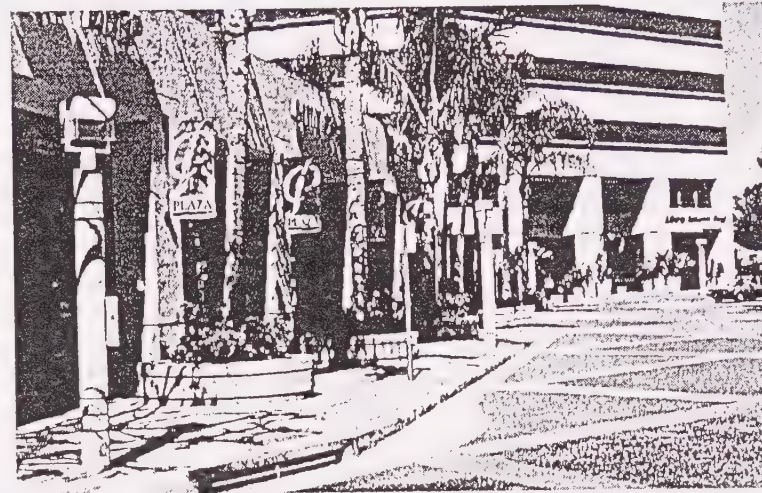
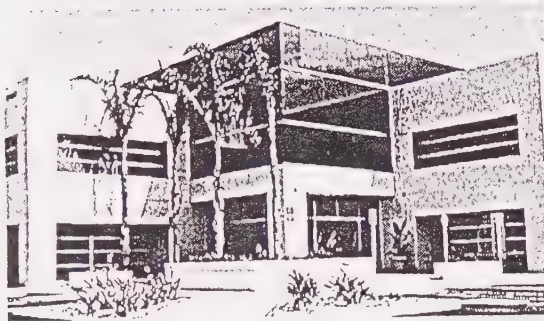
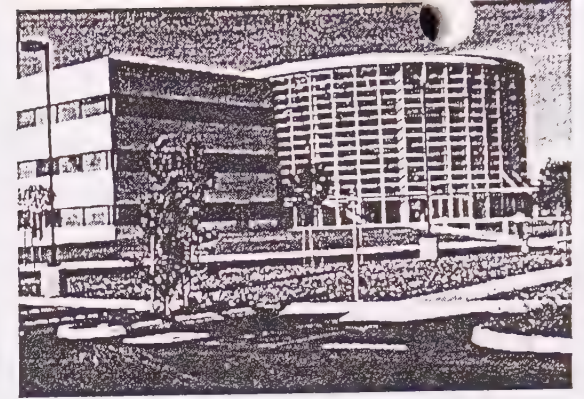
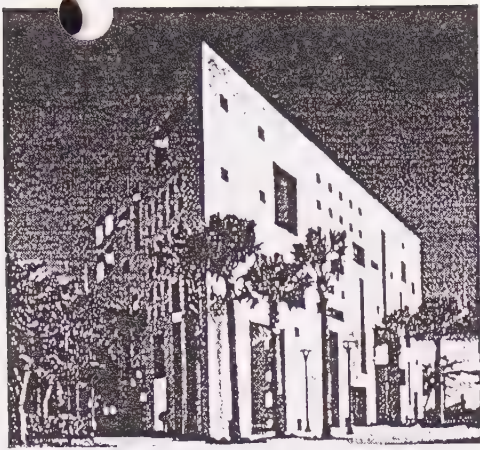
Exhibit 22



- 5.1.7 **Entry drives** shall be of a uniform size and design throughout the project area. Main and secondary entrances shall be thirty (30) feet wide. Loading area access drives shall be forty (40) feet wide (Exhibit 23).
- 5.1.8 **Handicap accessibility** shall be incorporated into all individual project plans and must reflect sensitivity to the needs and requirements of handicapped employees and visitors. The California Accessibility Code (Title 24) requirements shall be considered as a minimum set of guidelines. All accessways and parking layouts shall be handicap accessible and convenient.
- 5.1.9 **Energy conservation techniques** are encouraged and shall be provided on a project by project basis.
- 5.1.10 **Security provisions**, including lighting, building entrance visibility and drive locations, shall be carefully considered, and subject to review and approval of the Director of Public Works and the Police Department.
- 5.1.11 **Relationship to adjacent sites** shall be considered concurrent with individual project layout. Projects shall be "off-set" to minimize views directly into opposing buildings



Typical Entry Drive
Exhibit 23



Architectural Details

Exhibit 24

5.2 ARCHITECTURAL GUIDELINES

The Architectural Guidelines are intended to establish a compatible character, style and quality for all development projects within the McDonnell Centre Business Park. This compatibility of character is not intended to discourage individual innovation and creativity, but to simply provide a framework within which an overall sense of community and place will be reinforced.

The architectural theme shall reflect a contemporary research and development complex. Each project shall be designed and sighted with sensitive regard to climate, context, and proper use of materials and form in an honest expression of function as well as aesthetics. Building design shall comply with the following architectural policies.

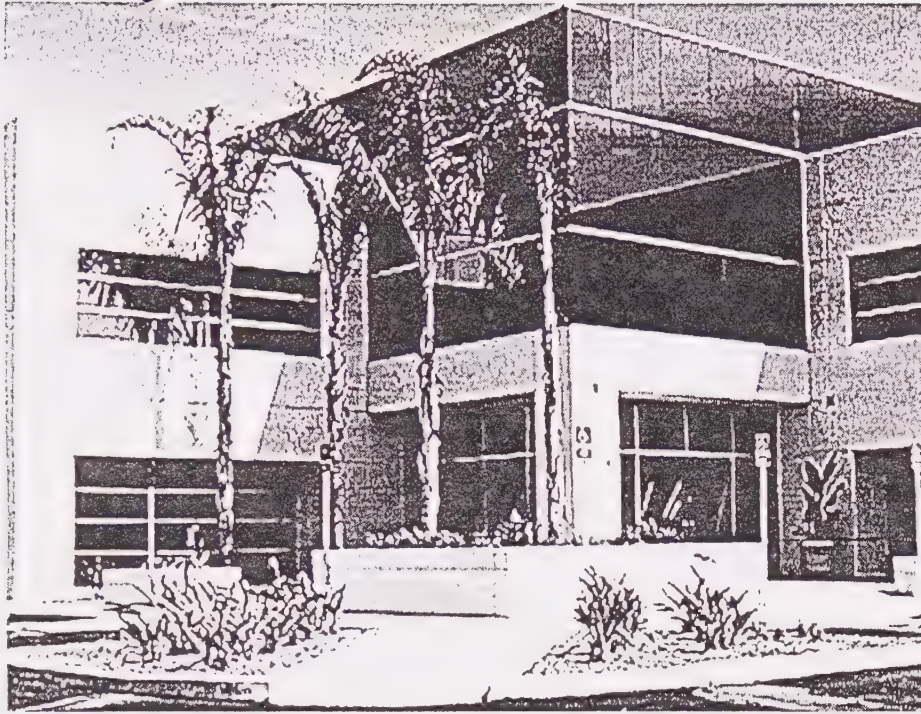


5.2.0 POLICIES: (All Buildings)

- 5.2.1 **Building location**, massing and orientation on the individual building sites, shall provide a balance in form and composition.
- 5.2.2 **Building massing** shall possess a balance in form and composition; large flat unarticulated building elevations shall not be permitted adjacent to a public street.
- 5.2.3 **Building articulation and fenestration** are required to avoid large flat building walls. Building elevations can also be enhanced with second story areas and/or vaulted areas establishing a variety in building volumes and composition (Exhibit 25).



Building Articulation
Exhibit 25



Typical Building Entry

Exhibit 26

- 5.2.4 **Building relationships** shall be designed with consideration to the proper screening of adjacent projects. Special attention shall be given to maintain the highest quality of design, harmony and compatibility.
- 5.2.5 **Building entries** shall be obvious. A clearly defined, primary pedestrian entry, with an enhanced hard-scaped foreground, is required for each building (Exhibit 26).
- 5.2.6 **Building materials, colors and finishes** shall be chosen from a palette of subtle tones (white, off white, light gray and beige); projects are encouraged to use color accents.

- 5.2.7 **Glazing** shall be tinted with high-performance materials (glazing colors, transparency and reflectiveness shall be limited to green, blue, and light gray shades. Clear or lightly tinted glazing is also acceptable).
- 5.2.8 **Mechanical equipment**, shall be screened from view of adjacent property. Mechanical equipment shall not be exposed on the wall surface or roof of a building. Screening material and color shall be compatible with the overall building design and colors, no lattice work is allowed. Backflow devices, electrical transformers and other mechanical equipment, located out of or within setback areas, shall be screened from public view or undergrounded, with the exception of public safety features.
- 5.2.9 **Exterior lighting** shall be located and designed to minimize direct glare beyond the parking lot or service area. Light standards under thirty-five (35) feet in height are recommended throughout a project area and shall illuminate all sidewalks and connecting walkways. All light standards shall be consistent with respect to design, materials, color and color of light, and with the overall architectural style of the project.
- 5.2.10 **Trash enclosures** for refuse containers and equipment shall be easily accessed by service vehicles and screened from public view. Refuse containers and equipment shall be located within a building's facade or within a screened enclosure, and reflect the architectural style of the main building, including the use of similar materials. Landscaping shall be provided on each side of all screened enclosures within parking areas.

POLICIES: (Industrial Buildings)

- 5.2.11 **Building design** shall avoid a single dominant mass. Substantial variations in massing should include changes in height and horizontal plane. The horizontal mass of the building elevations may be broken up with external treatment detached from the main building structure. Such massing breaks, include: columns, colonnades, trellises, wall segment textures, materials, pattern or color and enhanced landscape treatment. The extent of massing breaks and building projections shall relate visually to the overall scale of the building.



Typical Building Wall

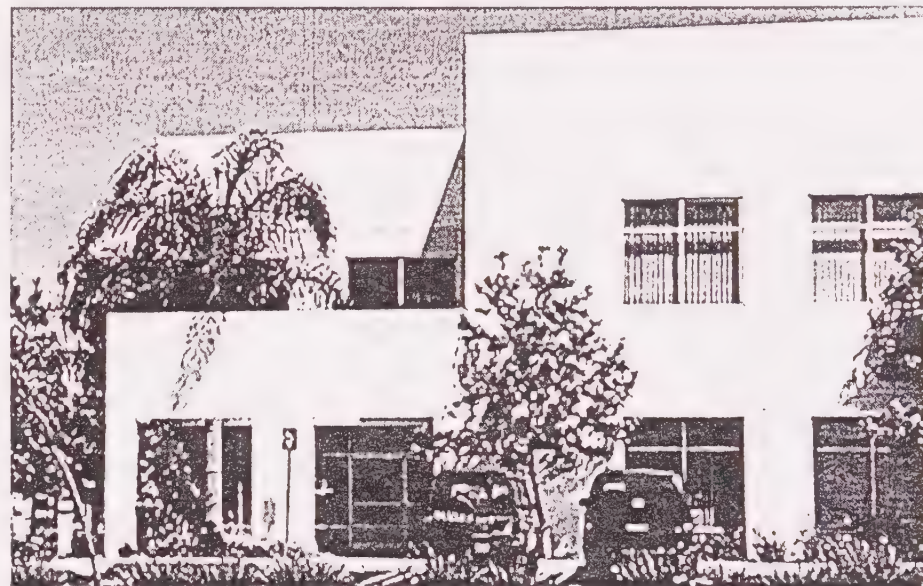
Exhibit 27

Blank walls shall avoid blank areas between massing breaks, especially along facades immediately visible from adjacent streets or walkways. This can be accomplished with a change in surface texture, revealed pilaster, a change in building planes, a vertical variation of the roof line, window placement and /or intensified accent landscaping (Exhibit 27)..

Building walls visible from public streets shall be especially attractive, blank flat facades shall be avoided.

These facades shall include a major entry feature and architectural variation over at least fifteen (15) percent of the facade's linear surface. The entry feature treatment shall be an integral part of the building design, a monolithic appearance shall be avoided (Exhibit 25).

Building corner situations shall incorporate special architectural treatment on elevations, visible from a public street. Any special facade treatment shall be continued around the building (Exhibit 28).

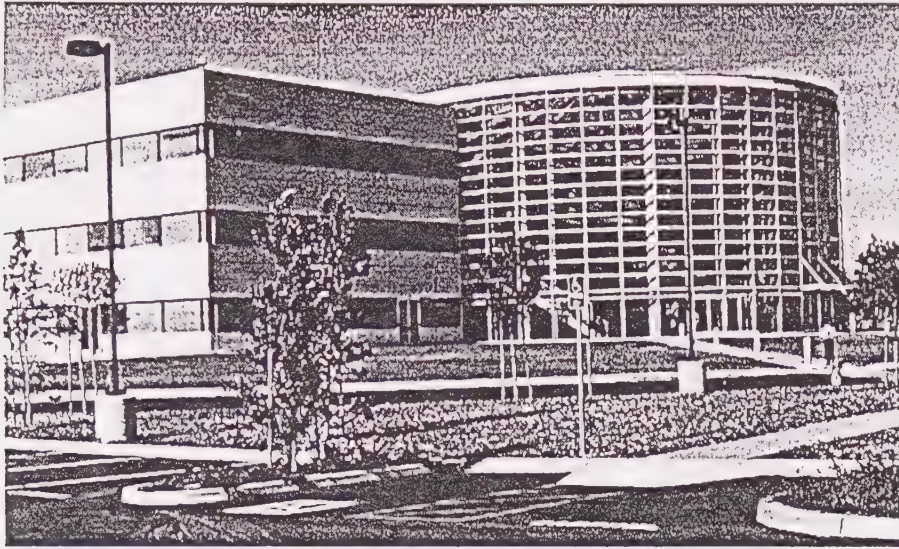


Building Corner Treatment

Exhibit 28

- 5.2.12 **Building entries** shall be emphasized by design features such as overhangs, recesses, walls and roof forms that are integrated into the overall building design. Greater height can be used to highlight and accentuate entries (Exhibit 26).





Building Materials Variety
Exhibit 29

5.2.13 **Building materials** may be a combination of concrete, metal, glass and/or other contemporary composites. Concrete tilt-up construction can be an integral component of building design (Exhibit 29).

5.2.14 **Mechanical equipment** such as ventilation devices, louvers, exposed flashing, tanks, overhead doors, and other service doors shall be finished consistent with the color scheme of the building. Cyclone blowers shall be screened and located below the fascia or roofline of a building. These devices shall be located at the rear and painted to match the color scheme of the building.

Mechanical equipment, located adjacent to but detached from the main building, shall be screened with compatible building and/or landscape materials.

5.2.15 **Satellite structures (detached)** can provide an effective variety in building layout and design; provided it can also be an efficient solution for company operations. Satellite structures must be designed to be compatible with the main structure and of the same materials.

5.2.16 **Loading areas** shall be designed to include attractive and durable materials, and conform with other guidelines pertaining to building features, materials and finishes. Fixed hardware for rolling doors shall be located on the inside of buildings to minimize visual clutter.

Loading docks and outdoor storage areas shall not face streets. These areas must be screened with a solid decorative wall or berm. Where views of these features are possible from streets or connecting walkways, they shall be screened through the use of walls, trellises, tall landscaping, or equivalent features. Loading docks shall be provided at a lower grade, where practical, to minimize views from the street and the need for tall walls or fencing (Exhibit 21).

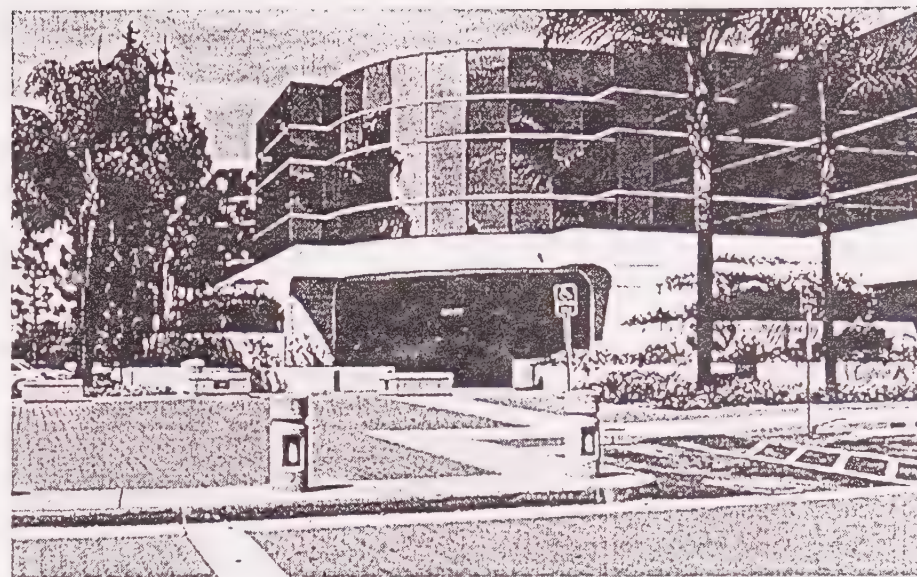
5.2.17 **Outdoor storage** shall not exceed ten (10) feet in height. All outdoor storage areas must be screened consistent with the loading area provisions. The wall height shall be sufficient to screen the loading areas and vehicles and trailers from view of adjacent properties and streets and shall not exceed ten (10) feet in height (Exhibit 21).

POLICIES: (Commercial/Office Buildings)

5.2.18 **Building designs** shall reflect an industrial/office theme and include a recognizable base and top. The base shall visually relate to the proportion and scale of the building. Contrasting materials, textures and color are encouraged on the base of buildings that face streets or connecting walkways, especially adjacent to major entries. Building rooflines are encouraged to take advantage of the visual prominence of a building's silhouette, office and entry area may include: cornice treatments, roof overhangs and brackets, richly textured materials, and/or different color of materials (Exhibit 30).

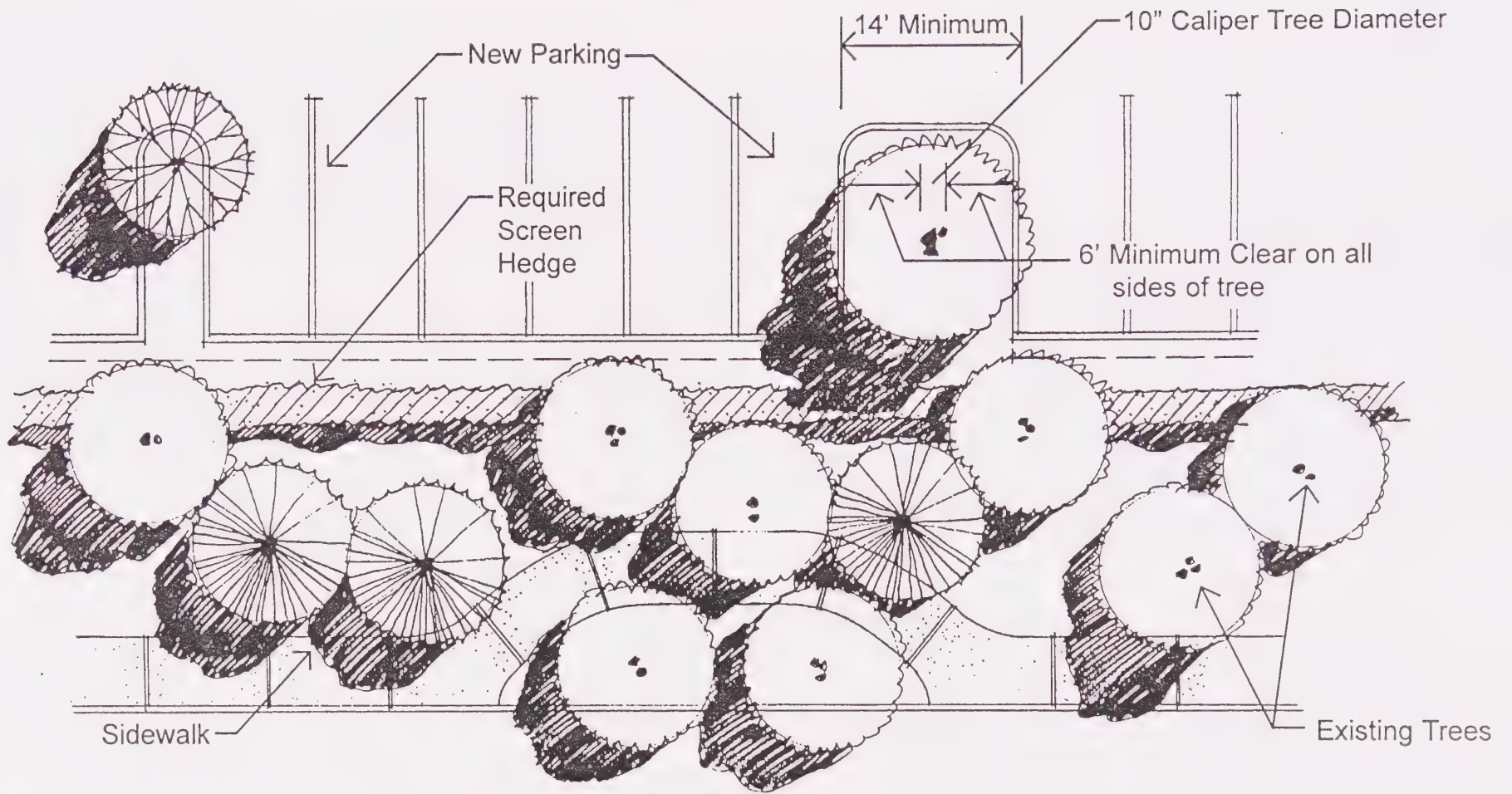


Commercial Facade
Exhibit 30



Office Facade
Exhibit 31

- 5.2.19 **Building materials** may be a combination of contemporary composites and compatible with the industrial buildings materials list (Exhibit 31).
- 5.2.20 **Illumination of buildings and landscaping** can be indirect, to create a strong positive image. Concealing light fixtures within buildings and landscaping can highlight attractive features. Use of lighting is especially recommended at entries, plazas, parking lots, and other areas where evening activity is expected.



Preservation of Existing Trees

Landscape Detail

Exhibit 32



5.3 LANDSCAPE GUIDELINES

The Landscape Concept for the McDonnell Centre Business Park is an integral component of the overall project design. The Landscape Concept is composed of several design and implementation elements; strong emphasis is to be placed on preservation of the existing landscape pattern. The Landscape Guidelines establish the design character and visual qualities for individual development in the project area.

The Landscape Guidelines provide uniformity to the site and establish a "Sense of Place" with both functional and aesthetic considerations. The Guideline proposes a continuation of the landscape and streetscape patterns currently surrounding the aerospace facility. These elements include intensified perimeter landscaping, large greenbelt areas, and pedestrian walkways which unify the project area. The landscape treatment for buildings and public right-of-ways should be designed with an internally consistent pattern and comply with the following landscape policies.

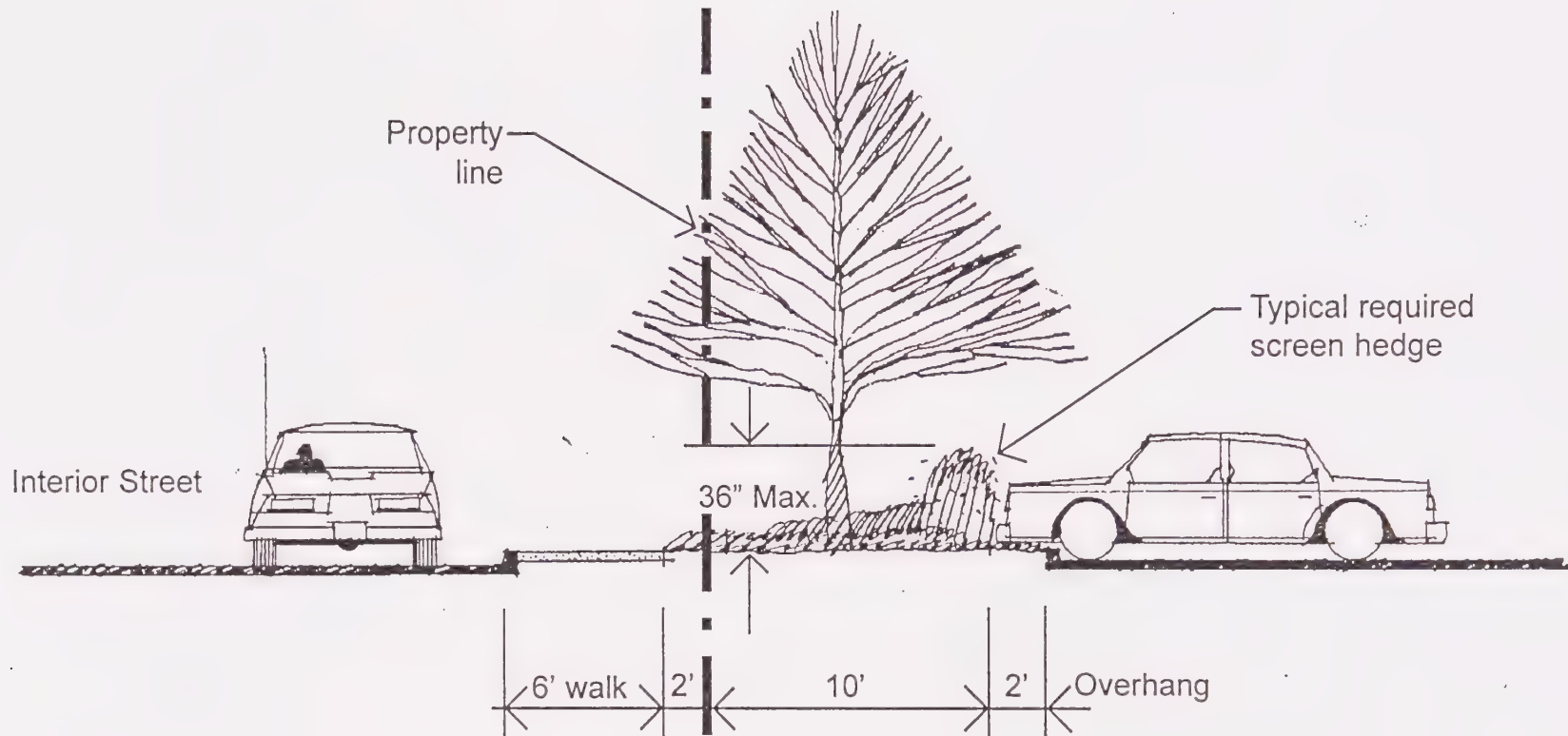
5.3.0 POLICIES:

5.3.1 **Site layout** shall respect and preserve the natural and existing site features, including trees and berming. A professional consulting arborist shall be used to determine how existing trees can be saved during design and construction (Exhibit 32).

5.3.2 **Existing healthy trees**, to the greatest extent feasible, shall be preserved and maintained on site. Where preservation is not possible, existing healthy trees should be relocated. If healthy trees are removed, replacement shall be as follows: Six (6) to ten (10) inch caliper - replace with two (2) thirty-six (36) inch box trees. Trees larger than ten (10) inch caliper, as measured at the breast height (4' - 6' above grade), shall be replaced with a single larger specimen tree, one (1) forty-eight (48) inch box and larger. All tree replacement shall be subject to review and approval and may be modified by the Director of Public Works (Exhibit 32).

5.3.3 **Landscape design** shall provide informal groupings of deciduous and evergreen shade trees, flowering shrubbery, and ground cover. Trees shall be of even size and shape at time of installation. Replacement trees shall be mature accent trees and compatible with the surrounding existing trees. A minimum of ten (10) percent of the net site area shall be landscaped. A maximum of five (5) percent of the landscape area may be improved with hardscape (entries, plazas, and walkways).





Typical Parkway Landscaping

Landscape Detail

Exhibit 33



- 5.3.4 **Plant materials** shall be selected to create an informal pattern of landscaping to reinforce the character of the existing tree plantings. A more formal pattern of landscaping shall be created on-site at the project entries. Trees shall be selected based upon the size of the planting area, to allow for mature growth without causing future damage to the improvements.

All trees shall be a minimum twenty-four (24) inch box size. Shrubbery (evergreen and flowering) shall be low to medium in height grouped in informal masses, minimum size shall be five (5) gallon. All grass selections shall be made from the approved water efficient materials list (Exhibit 38).

- 5.3.5 **The right-of-way area**, shall provide a six (6) foot wide sidewalk, adjacent to the curb with a two (2) foot wide landscape strip between the property line and back of sidewalk. The landscape strip shall be adjacent to the individual projects landscaping and compatible to blend in with the on-site perimeter planting palette.

Tree planting in these parkway areas shall include a minimum of one (1) twenty-four (24) inch box tree for each twenty (20) feet of lineal frontage. Tree planting may include a combination with thirty-six (36) inch or larger box trees. Tree planting shall be grouped in informal drifts and tree quantities shall be determined by the length of the property adjacent to the street divided by the recommended spacing of each tree variety. All parkway planting shall be subject to review and approval of the Director Of Public Works (Exhibit 33).

- 5.3.6 **Pedestrian walkway systems** shall be designed to unify the entire project area and provide pedestrian site access to buildings, parking and site activity areas. Pedestrian walkways shall be a minimum of six (6) feet in width.

Pedestrian walkways shall exist on each parcel and within the adjacent public right-of-way. All landscaping adjacent to the pedestrian walkway improvements shall be installed concurrent with each individual project and consistent with the Landscape Concept (Exhibit 33).

- 5.3.7 **Perimeter landscaping** around the project areas shall provide a consistent edge treatment using a limited number of plant materials, and shall meander in informal groupings around the site. An area shall be preserved for a six (6) foot wide sidewalk along this perimeter edge within the public right-of-way and shall be consistent with the edge treatment on all perimeter streets (Exhibit 33).



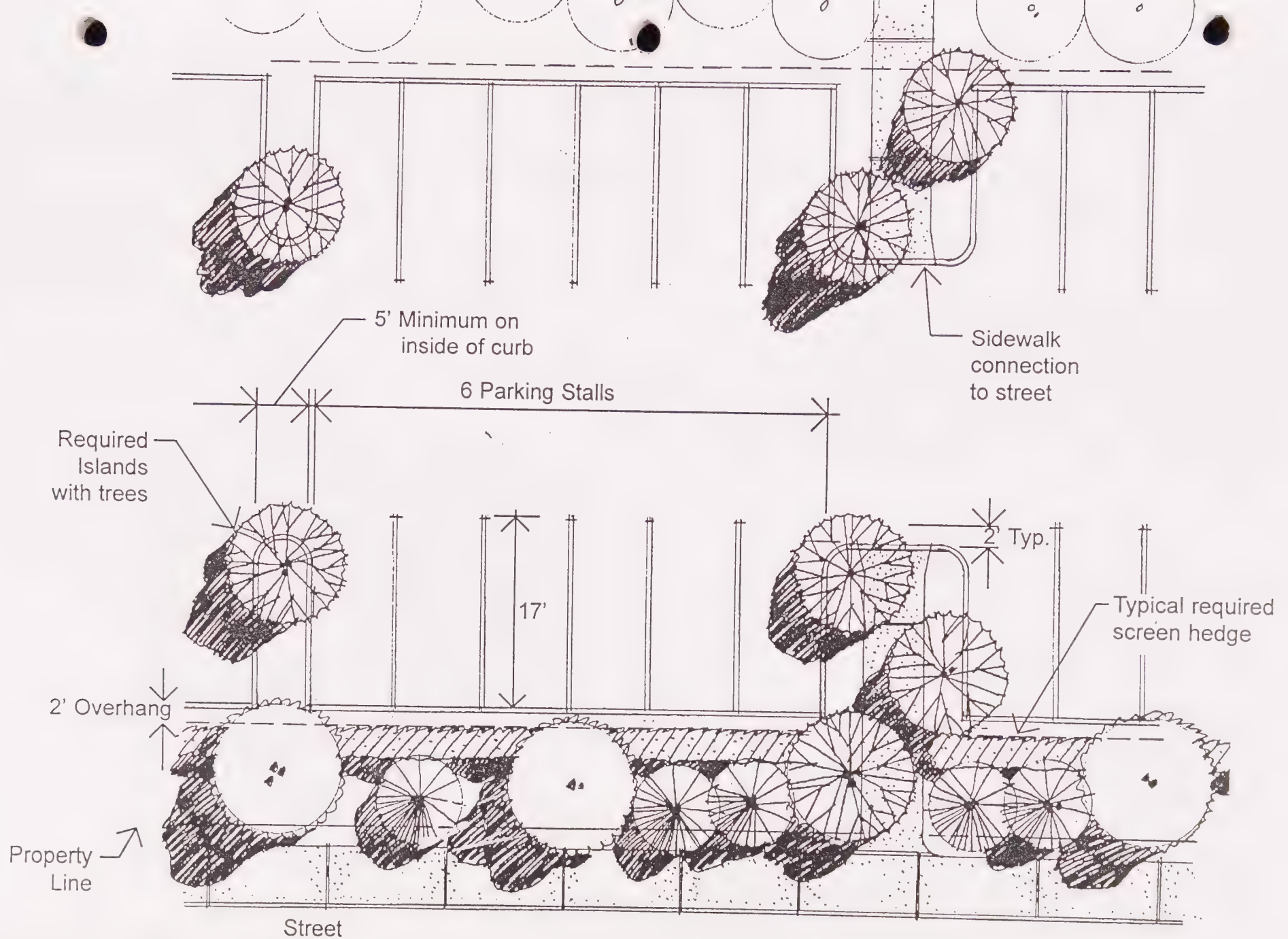


Exhibit 34

Typical Parking Lot Landscaping



- 5.3.8 **Parking lots** shall be planted at a rate of one (1) tree for every six (6) parking stalls. Parking areas immediately adjacent to public streets shall provide one (1) tree in a landscape area between each sixth parking stall. Parking lot trees shall be twenty-four (24) inch box trees. All tree planting areas shall be a minimum net width of five (5) feet (Exhibit 34).

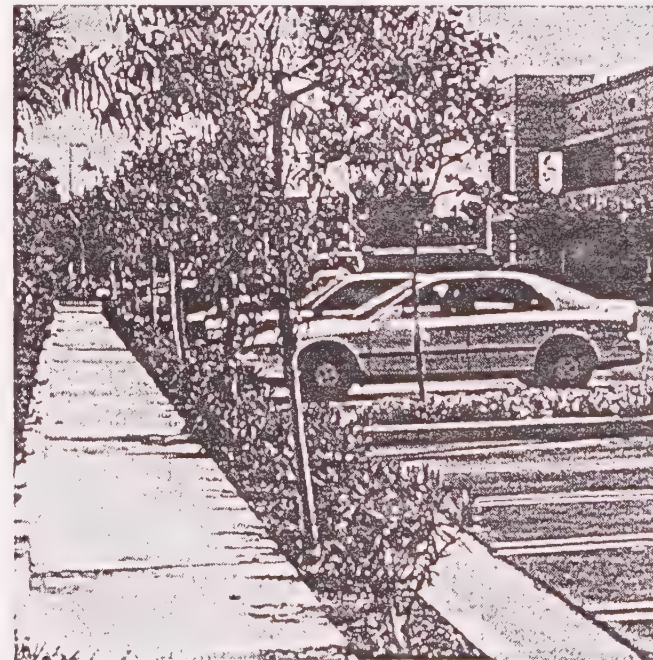
Parking lot treatments shall be consistent and contribute to the project landscaping unity. Parking lots shall be planted with trees in such a manner as to provide maximum shade. An alternative which clusters or groups parking lot trees adjacent to the building may be considered. Larger trees may also be substituted for a number of smaller trees, subject to review and approval of the Director of Public Works.

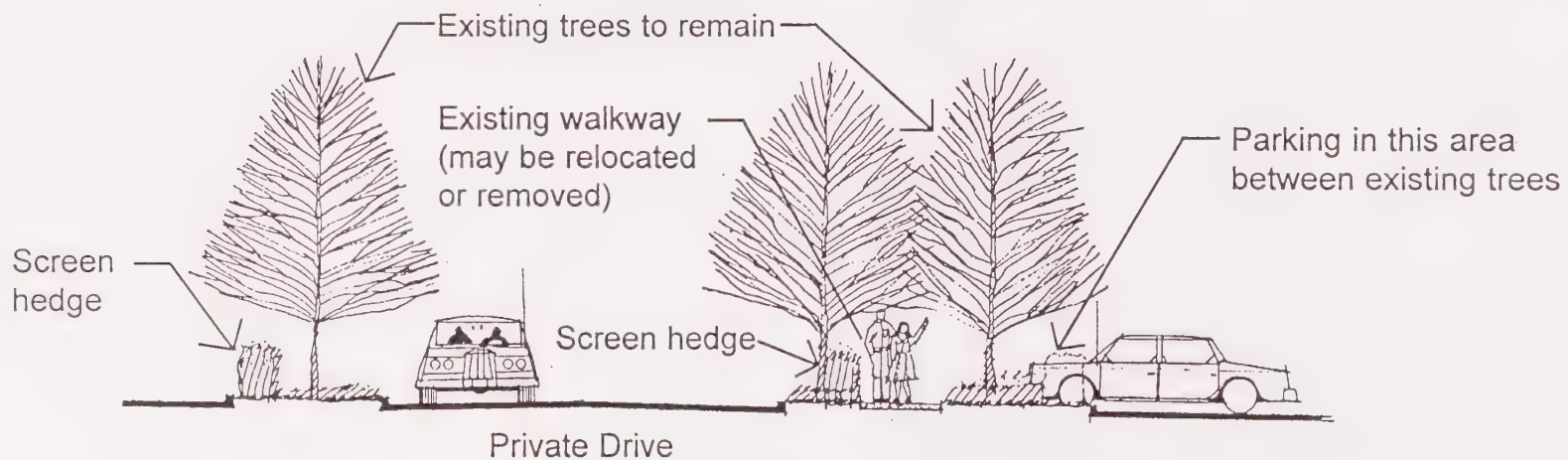
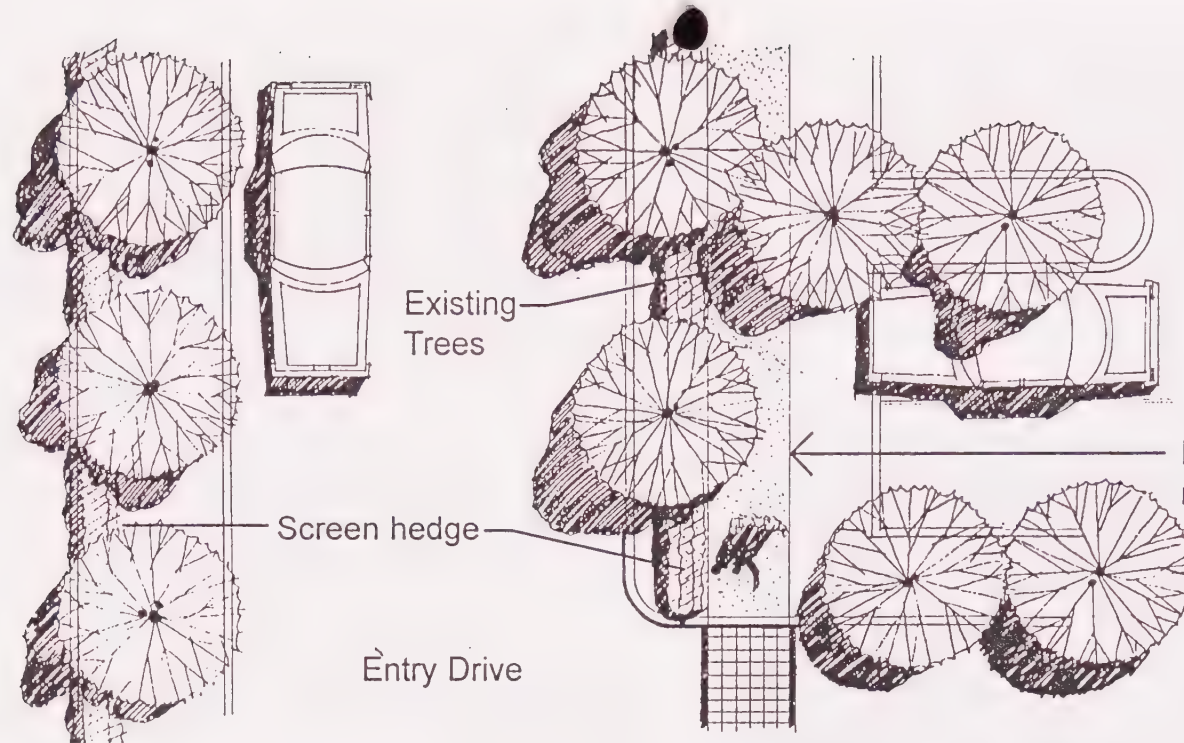
Perimeter parking lots adjacent to arterial streets, shall be provided with additional landscape treatment to ensure that the parking areas are adequately screened from adjacent street views. Berming in these areas is encouraged and shall be a maximum of three (3) feet high and have a natural appearance in form.

Shrubbery shall be planted in areas where berms are not practical, along the perimeter of the parking areas. Shrub planting shall be provided in a minimum five (5) gallon size and spaced a maximum of three (3) feet apart. Shrubbery shall not exceed three (3) feet in height. Hedges shall be trimmed up from the ground and maintain an eight (8) inch clearance from the ground.

Where cars overhang the curbs, ground cover planting shall be required at a minimum width of three (3) feet (inside dimension). The overhang area shall not be considered as part of the required minimum percentage of on-site landscaping (Exhibit 34).

- 5.3.9 **Interior property lines** shall be planted with a continuous hedge, five (5) gallon shrubs at a minimum spacing and twenty-four (24) inch box trees; a minimum of one tree per twenty-five (25) lineal foot of property line.





Preservation of Existing Landscaping

Landscape Detail

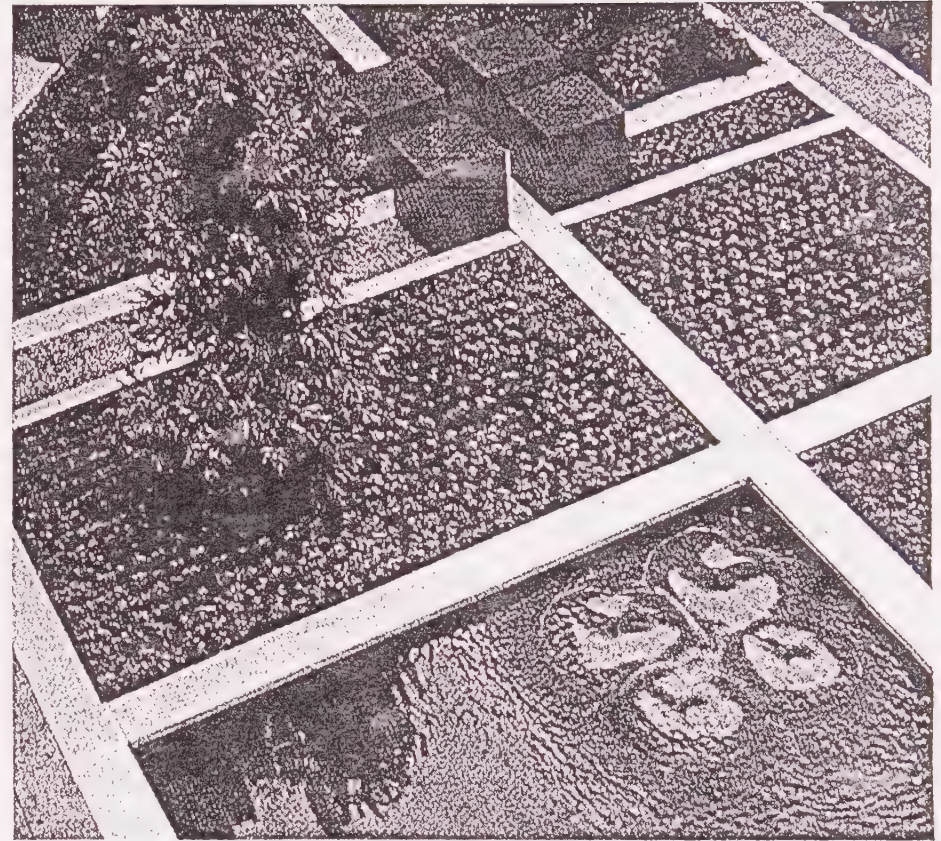
Exhibit 35

5.3.10 **Entry drives** shall be constructed in conformance with the Specific Plan (Policy 5.1.7) and City design standards (Public Works Standard Plans), subject to the review by the Director of Public Works. Project access points shall be designed to provide entering and exiting drives with adequate views of approaching pedestrians and vehicles (Exhibit 23).

Entry drives shall provide convenient access to parking lots at various site locations. In addition to street trees and on-site landscaping, each entry shall be designated by ground cover and one or two large thirty-six (36) inch box specimen trees, on both sides of the entry. These trees shall be located a minimum of ten (10) feet back from the face of the street curb to avoid the line-of-sight concern (Exhibit 35).

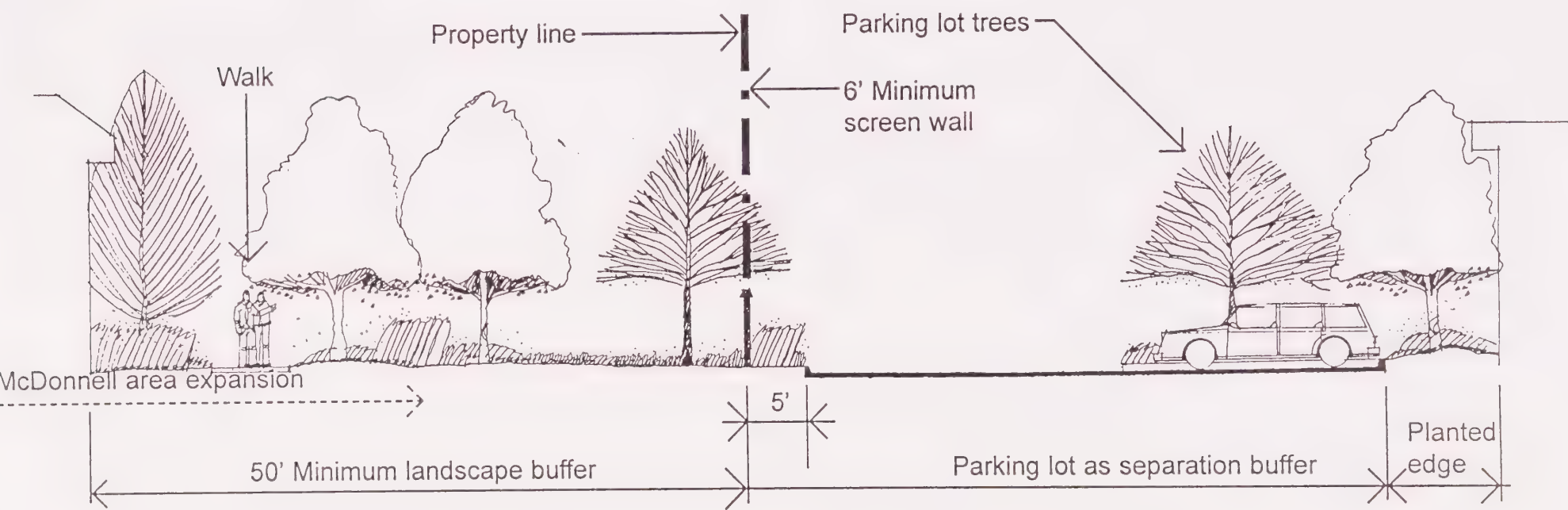
Stamped concrete, colored to match paving materials, shall be provided at all driveway entrances and all pedestrian connections from the main building to the public pedestrian walkway system, across drive aisles and through landscaping.

5.3.11 **Entry plaza areas, and courtyards** shall be provided as focal points and for employee use. These areas shall be an integral part of the building architecture and be connected by a walkway system to the public pedestrian walkways.



5.3.12 **Landscape buffer** areas shall be provided along the abutting edges between Planning Areas, concurrent with new development. These buffer areas are intended to provide for an aesthetic transition between the aerospace facility and any non aerospace new development. Landscape buffer areas shall be a minimum

of fifty (50) feet in width and shall include landscaping and berming to adequately screen adjacent uses, consistent with the established landscape theme. The buffer areas may include walls, fencing, utility easements and pedestrian walkways compatible with adjacent development.



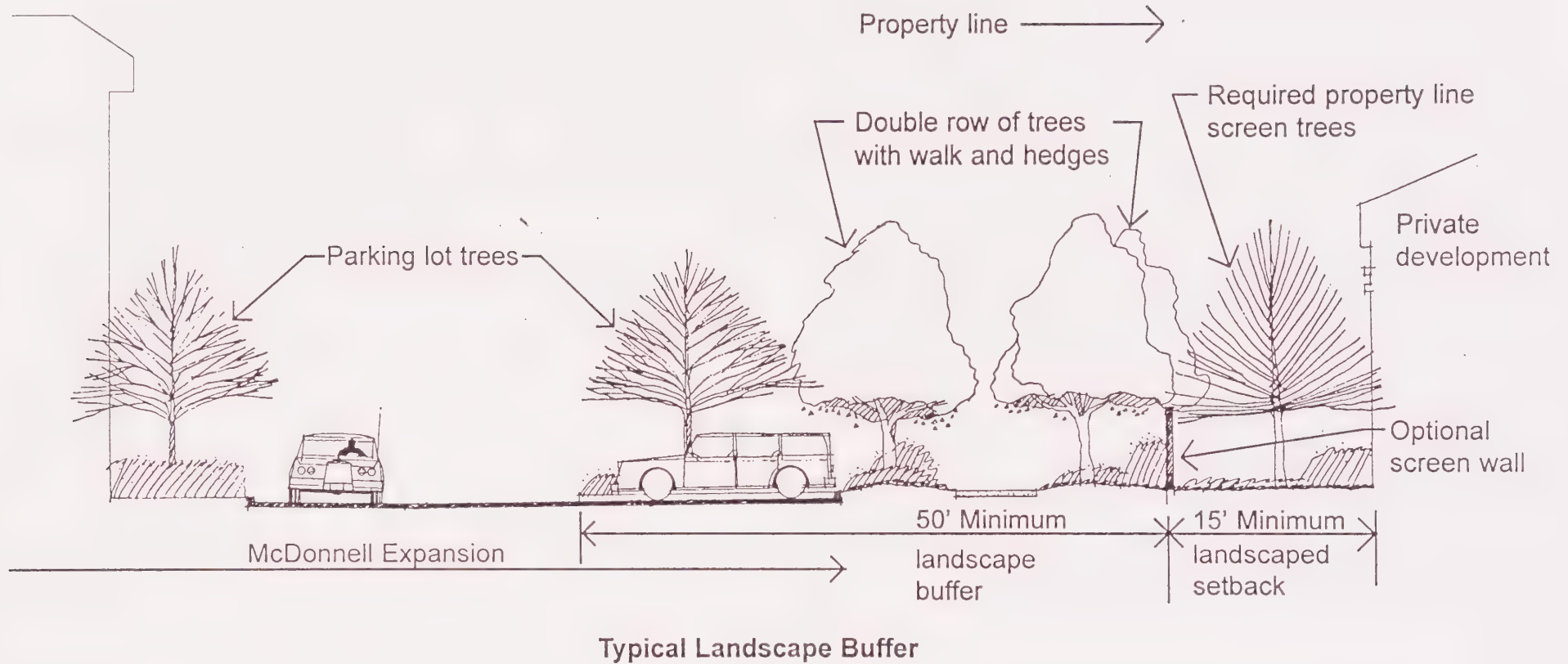
Typical Landscape/Parking Lot Buffer

Landscape Detail

Exhibit 36



Landscape buffer areas may also be used for a private access drive and/or parking lot provided an intensified landscape design is proposed. An additional building setback may also be necessary in order to adequately screen the adjacent use.



Landscape Detail

Exhibit 37

PLANT PALETTE

Street Trees

1. Pinus Halepensis @ 35' oc.
Canariensis @ 20' oc.
Eldarica @ 20' oc.
2. Liquidambar Styraciflua 'Palo Alto' @ 25' oc.
3. Podocarpus Gracilior @ 30' oc.
4. Eucalyptus Citriodora @ 20' oc.
Camaldulensis @ 20' oc.
Maculata @ 20' oc.
Ficifolia @ 20' oc.
5. Agonis Flexvosa @ 20' oc.
6. Cinnamomum Camphora @ 20' oc.

Accent Trees

1. Lagerstroemia Faurei
2. Prunus Cerasifera 'Thundercloud'
3. Eucalyptus, species
4. Erythrina Caffra
Coralloides
5. Brachychiton Acerifolius

Property Line Trees

1. Eucalyptus Ficifolia
Nicholi
2. Podocarpus Gracilior
3. Agonis Flexuosa

Tall Screen Hedges (5 gal min)

1. Ligustrum Texanum @ 30" oc.
2. Nerium Oleander @ 36" oc.
3. Xylosma Senticosa @ 36" oc.
4. Leptospermum Scoparium @ 36' oc.
5. Prunus Caroliana @ 36" oc.

Low Screen Hedges (5 gal min)

1. Carissa Grandiflora @ 36" oc.
2. Nerium Oleander Petite Pink @ 36" oc.
3. Juniperus Tamarisifolia @ 36" oc.

Ground Cover

1. Mesembryanthemum Rosea
2. Gazania Mitsuma
3. Grass Festuca/Marathon II or III

Plant Materials Palette

Exhibit 38



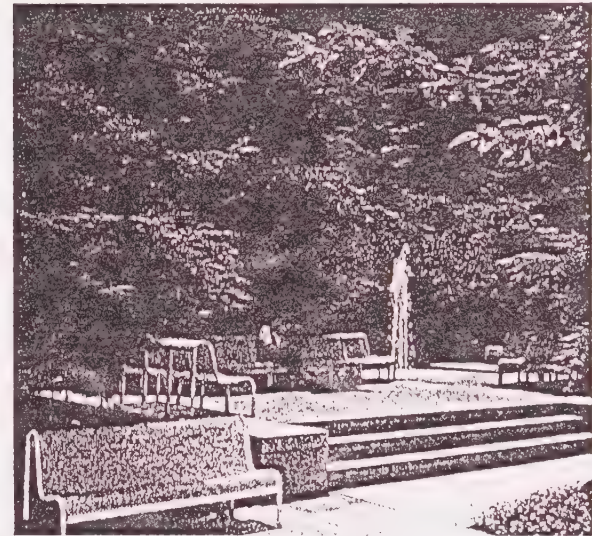
- 5.3.13 **Landscape screening** is intended to soften and blend the connection of the developed building areas with the landscape of the surrounding sites. This connection shall be made with compatible ground cover, shrubbery and trees. Trees shall be provided to soften, and visually relieve, building elevations and to provide summer shade.

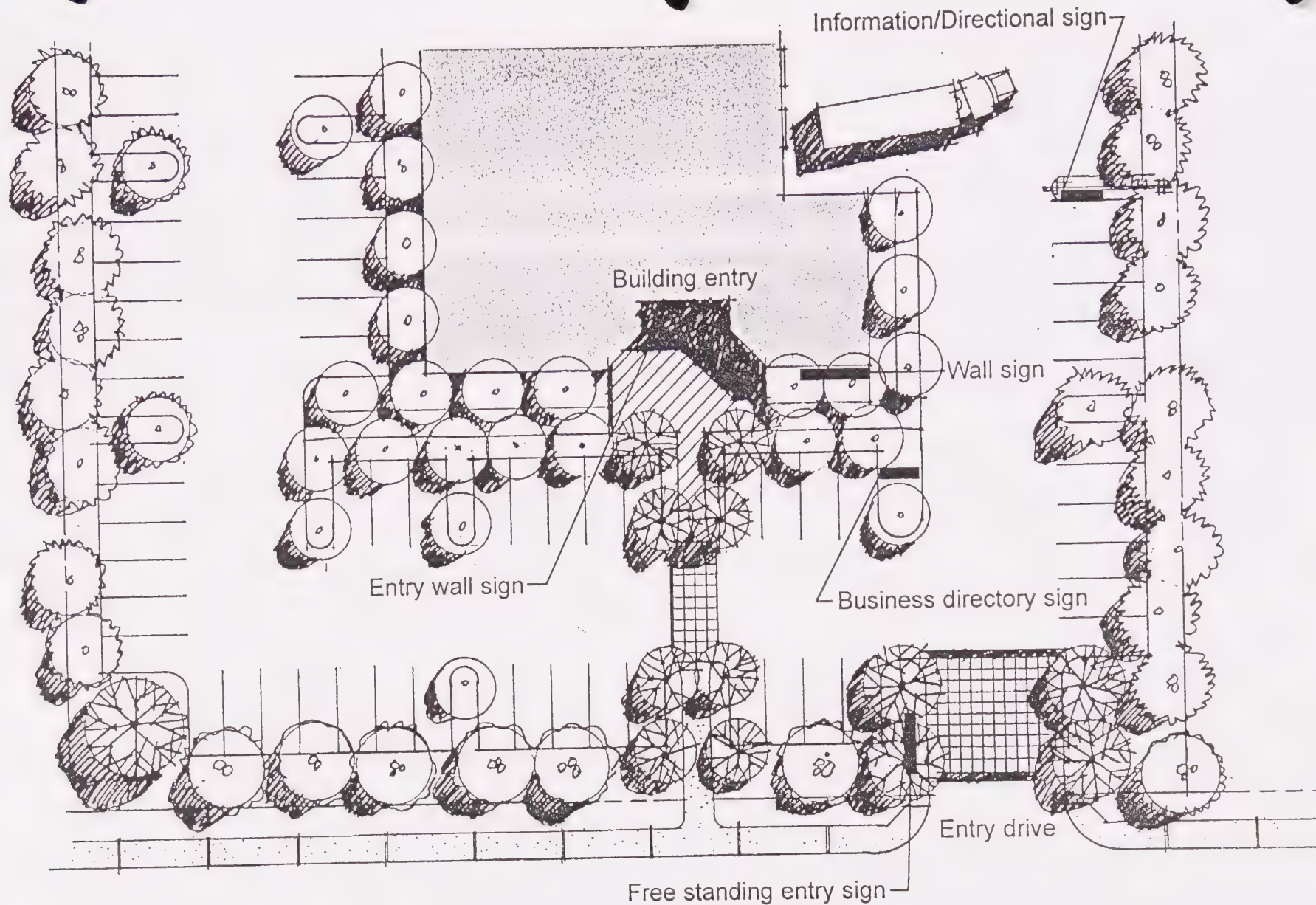
Trash enclosure areas shall be provided with tree and shrub planting screens to soften the enclosure. Mechanical equipment and transformer areas shall have landscape screening and/or low-level screen walls. Valves, meters, back flow preventers, etc., shall also be screened by shrub plantings and/or low level screen walls.

- 5.3.14 **Landscape lighting** shall be provided to aesthetically enhance the site, as well as providing for the safety and security of motorists and pedestrians throughout the project area.

Pedestrian walkways shall include adequate night lighting for public safety.

- 5.3.15 **Conservation** water measures shall be incorporated in the landscape design. A minimum of seventy-five (75) percent of the required landscape area shall be planted with ground cover and the balance with turf. The use of shrubs, hedges, and berming, shall be provided to screen cars in the parking lots from street view.





Typical Signage Layout

Sign Detail

Exhibit 39



5.4 SIGNAGE GUIDELINES

The Signage Guidelines identify a framework in which advertising a place of business, providing directions or information can be accomplished without detracting from the overall design quality of the project area. The Signage Guidelines also contribute to the overall project area design theme. Design, color, materials and placement are all important in creating signs that are architecturally attractive and integrated into the overall project area design. The intent is to create and promote a quality visual environment by allowing only signs which are compatible with their surroundings and which effectively communicate their message.

This section sets forth general criteria for all signs within the project area. Signs shall be designed to be architecturally compatible with the colors and materials of the adjacent building. All signing shall be subject to the provisions and procedures of the Huntington Beach Zoning and Subdivision Ordinance, and comply with the following policies.

5.4.0 POLICIES:

5.4.1 **Wall signs** and logos attached to the building shall be individual letters and surface mounted. No signs shall be painted directly on the building. No signs shall be boxed with internal lighting and attached to a building.

5.4.2 **Wall signs** shall be located on the building for optimum visibility from the adjacent street.

5.4.3 **Wall signs** shall only be located on the building face adjacent to a street and shall be limited to identify tenants within each building. These signs shall be restricted to the name of the firm, company or corporation only. The colors and materials of the sign structure shall be compatible with the building architectural colors and materials, sign face materials and colors may contrast.

5.4.4 **Wall signs** shall not exceed an area equal to one square foot for each lineal foot of building frontage. Maximum size sign for each building facade adjacent to a street shall be one-hundred (100) square feet for industrial projects, two-hundred (200) square feet for commercial projects. Maximum letter height shall be thirty (30) inches.

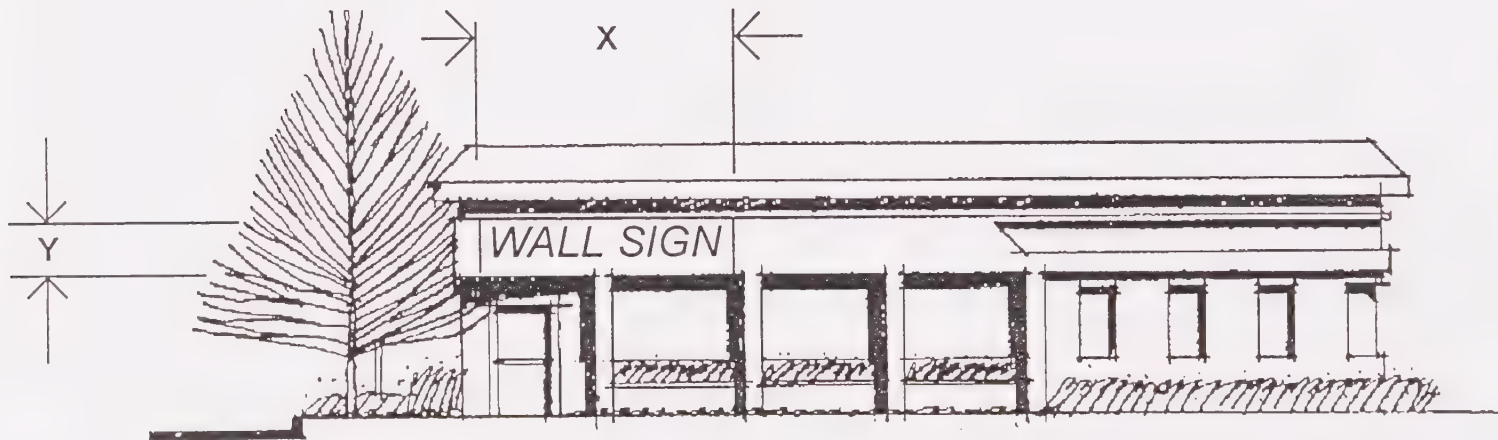
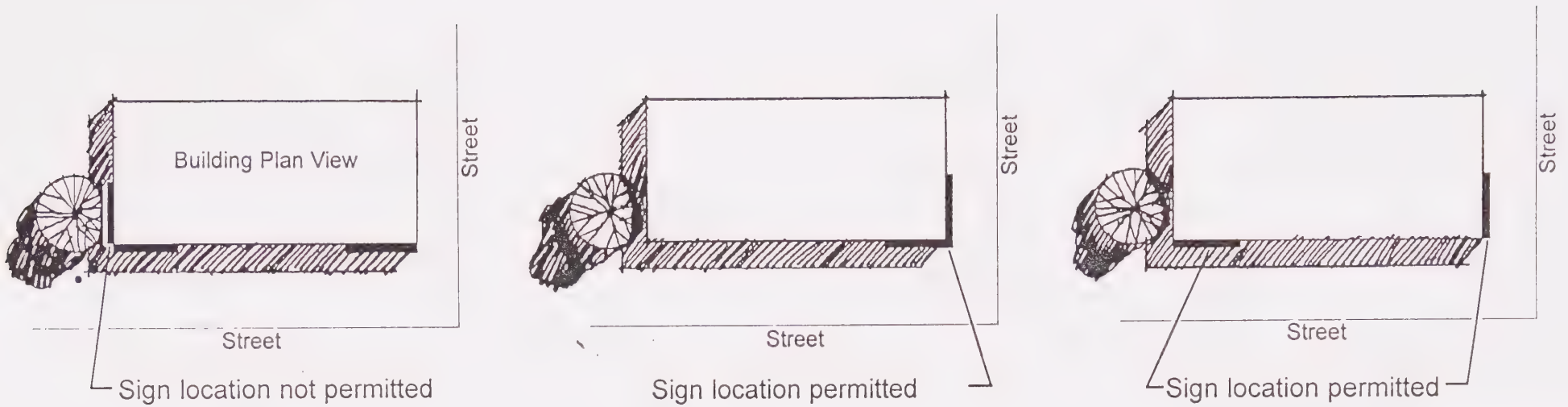
5.4.5 **Wall signs** shall be limited to one (1) sign per building elevation with a maximum of two (2) wall signs per primary tenant on non-adjacent building facades. These signs shall be internally illuminated or non-illuminated.

Wall Sign Chart

	Maximum Number	Maximum Area	Other
Commercial	1/Business	200 sq. ft	<ul style="list-style-type: none">• Below roof line• Channel letters only• Raceways not Permitted
Industrial	1/Business/ street frontage	100 sq. ft.	
Office	1/Business	100 sq. ft.	

Exhibit 40

Wall Sign Location



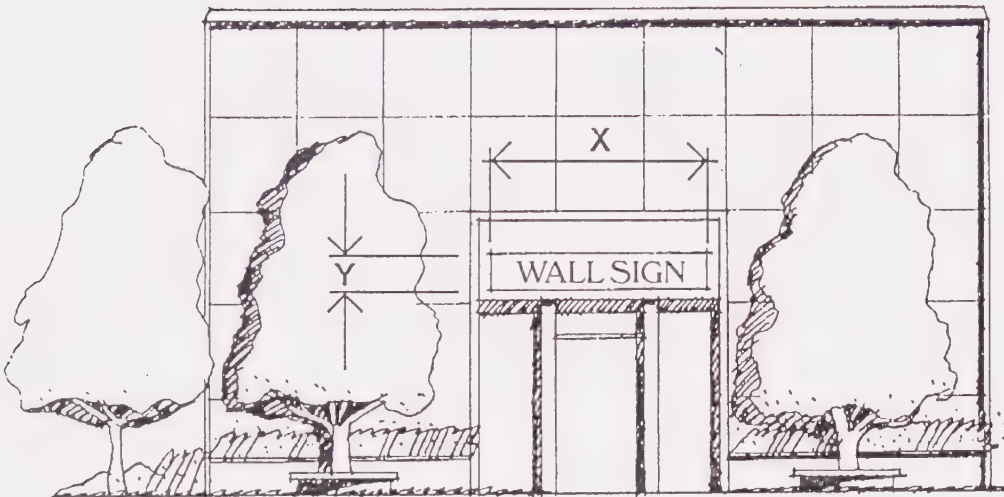
Wall Sign Detail

Exhibit 41

Dimensions of X and Y varies.
 X times Y is not greater than 100 sq.ft. for
 industrial projects and 200 sq.ft. for
 commercial projects.

5.4.6 **Wall signs** shall be limited for secondary tenants to a maximum of one (1) sign per tenant. In no event shall there be more than four (4) secondary tenant wall signs permitted per building and no more than two (2) per building elevation.

5.4.7 **Additional wall signs** shall be permitted at the primary entry to a building. Maximum size shall be ten (10) square feet with the maximum letter height of twelve (12) inches.



Dimensions of X and Y varies.
X times Y is not greater than 10 sq.ft.

Entry Wall Sign
Exhibit 42

5.4.8 **Freestanding signs** for business identification shall be limited to perimeter locations adjacent to existing arterials and shall be of a monument design. These signs shall not exceed seven (7) feet in height, as measured from the adjacent grade, and not more than fifty (50) square feet in area for industrial and office and one hundred (100) square feet for commercial.

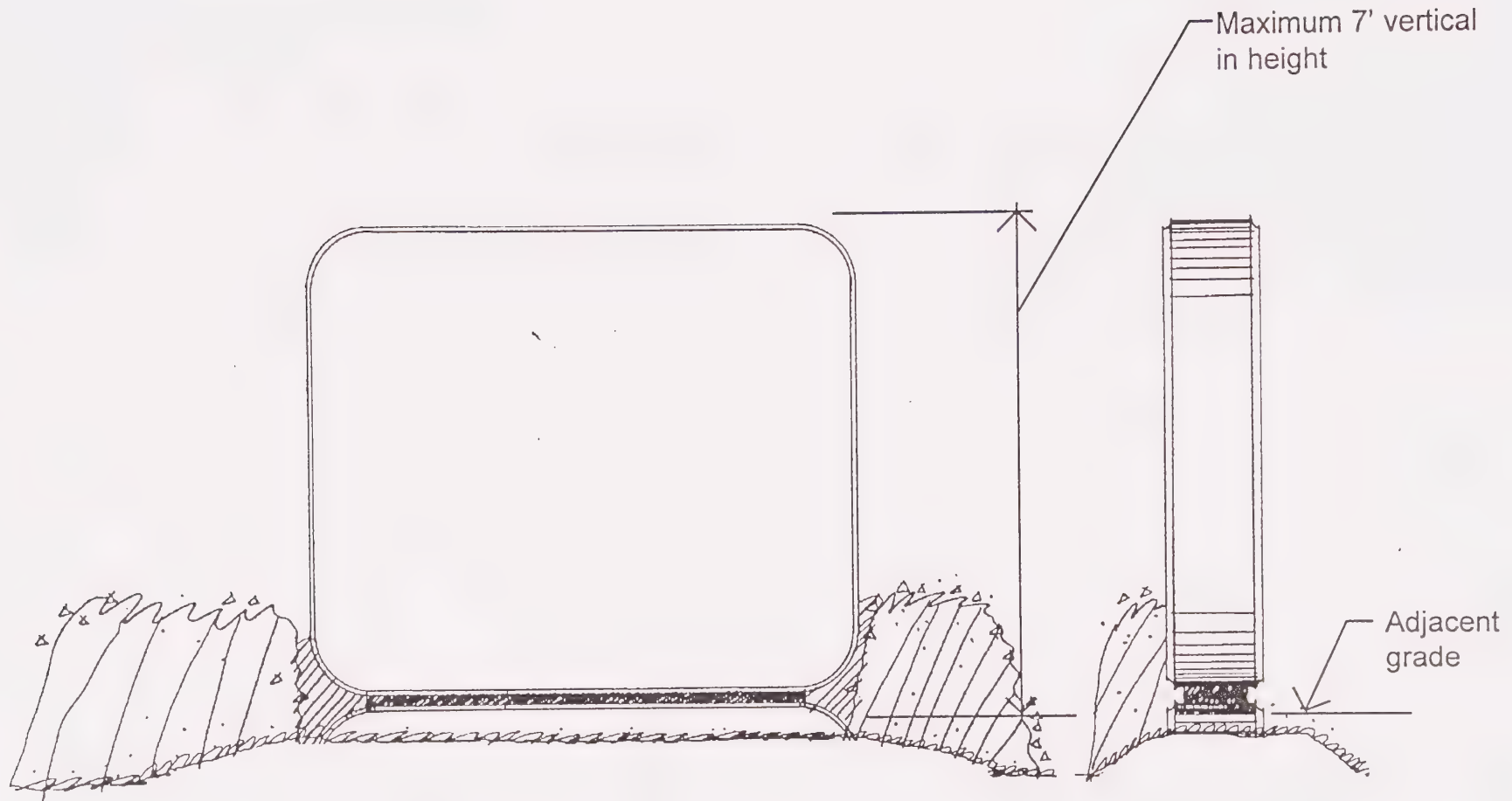
5.4.9 **Freestanding signs** for tenant identification may be installed within or adjacent to a private entry driveway. Entry signs must be located and sized so as to not interfere with vehicular visibility and/or movement. Entry signs shall be limited to thirty-two (32) square feet and shall not exceed four (4) feet in height. Entry signs may be placed on a berm not exceeding eighteen (18) inches in height. Entry signs shall include the building address.

Freestanding Sign Chart

	Maximum Number	Maximum Area	Maximum Height	Other
Commercial	1/250' of frontage	100 sq. ft.	7 ft.	Multi-tenant panels are permitted
Industrial and Office	1/building	50 sq. ft. 7ft.	7 ft.	Multi-tenant panels are not permitted

Note: Entry signs, information/directional signs and temporary signs are subject to separate regulations

Exhibit 43

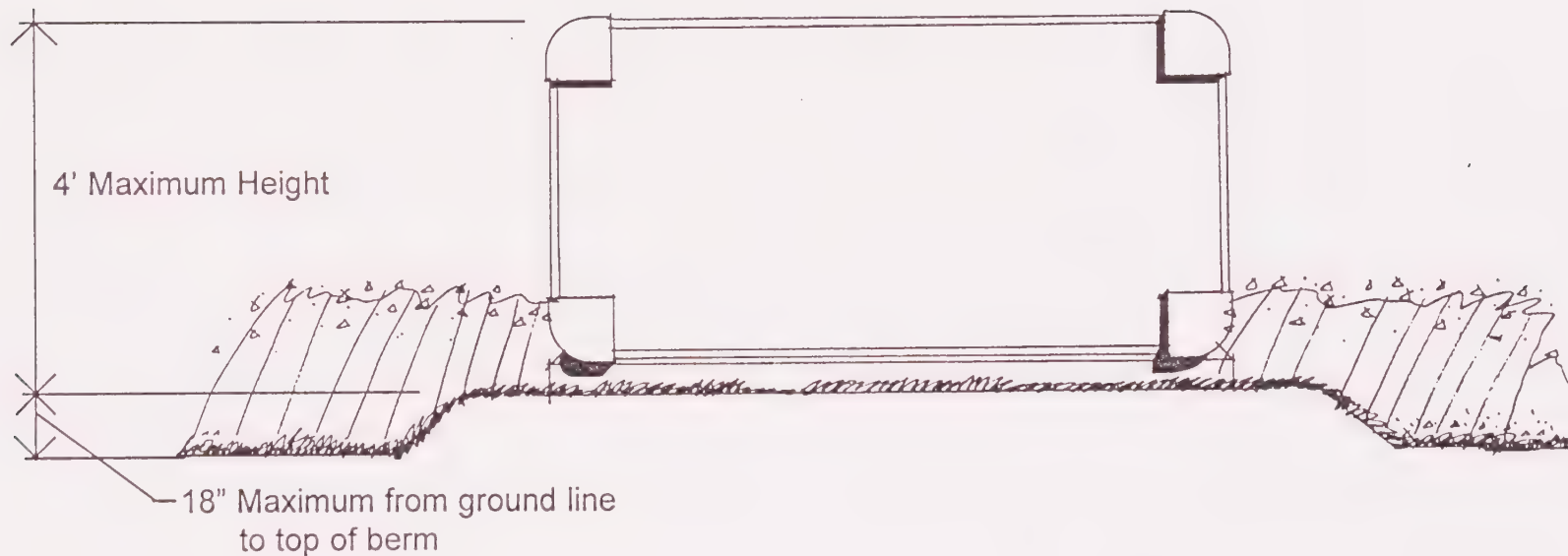


Freestanding Sign Detail

Exhibit 44



- 5.4.10 **All freestanding signs** shall be of a monument design, including: business identification, business directory, and information/directional identification. Street-side signs, at access driveways, shall be used to identify a building address/tenant, and to direct traffic to that building. Internal, on-site signs shall be utilized to provide information and direction to pedestrian and automobile traffic. Freestanding retail commercial signs shall be a minimum of two hundred and fifty (250) feet apart.



Entry Sign Detail

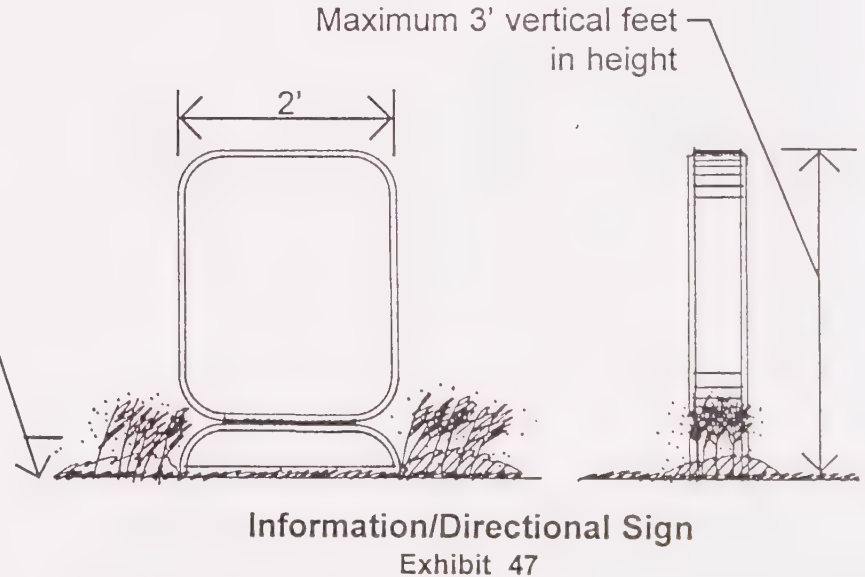
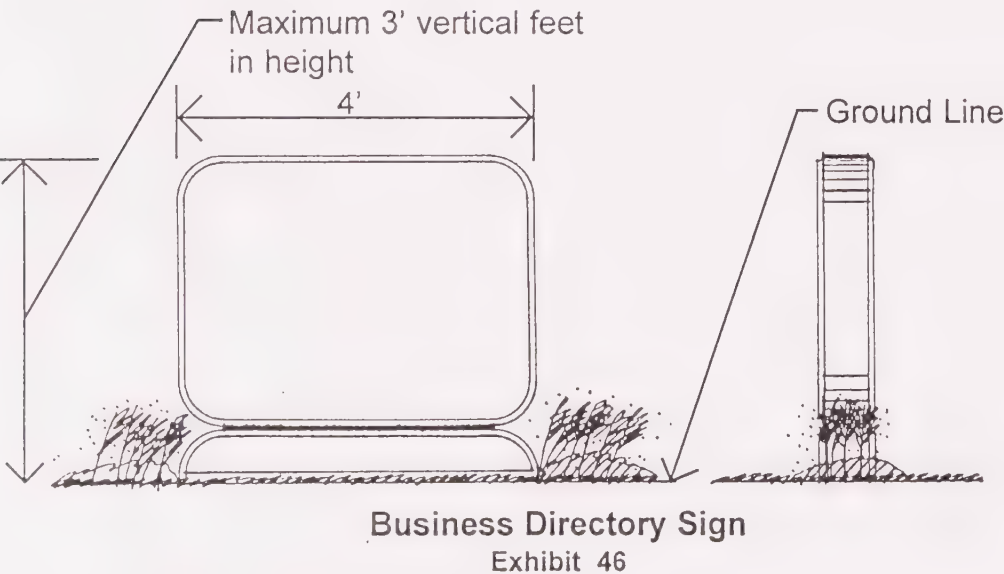
Exhibit 45

5.4.11 **Business directly signs** which are freestanding shall be located near the primary entry and access drive. These signs shall be limited to identifying the building address and tenants, and shall be visible from the intersection of a private driveway and a public street. These signs shall not be allowed elsewhere within the landscape setback area or along street frontages.

5.4.12 **Business directory signs** shall not exceed a maximum area of twelve (12) square feet per face, may be double faced adjacent to a street front and single faced adjacent to a building, and shall not exceed three (3) feet in vertical height. Signs shall be limited to one sign per building. More than one building address and tenant may be identified on one sign. Signs shall be consistent with the overall building architecture.

5.4.13 **Information/directional signs** shall be used to provide direction to on-site automobile traffic or pedestrians and not visible from off-site areas. Informational/directional signs shall be limited to six (6) square feet per face, double faced. This sign shall not exceed three (3) feet in vertical height.

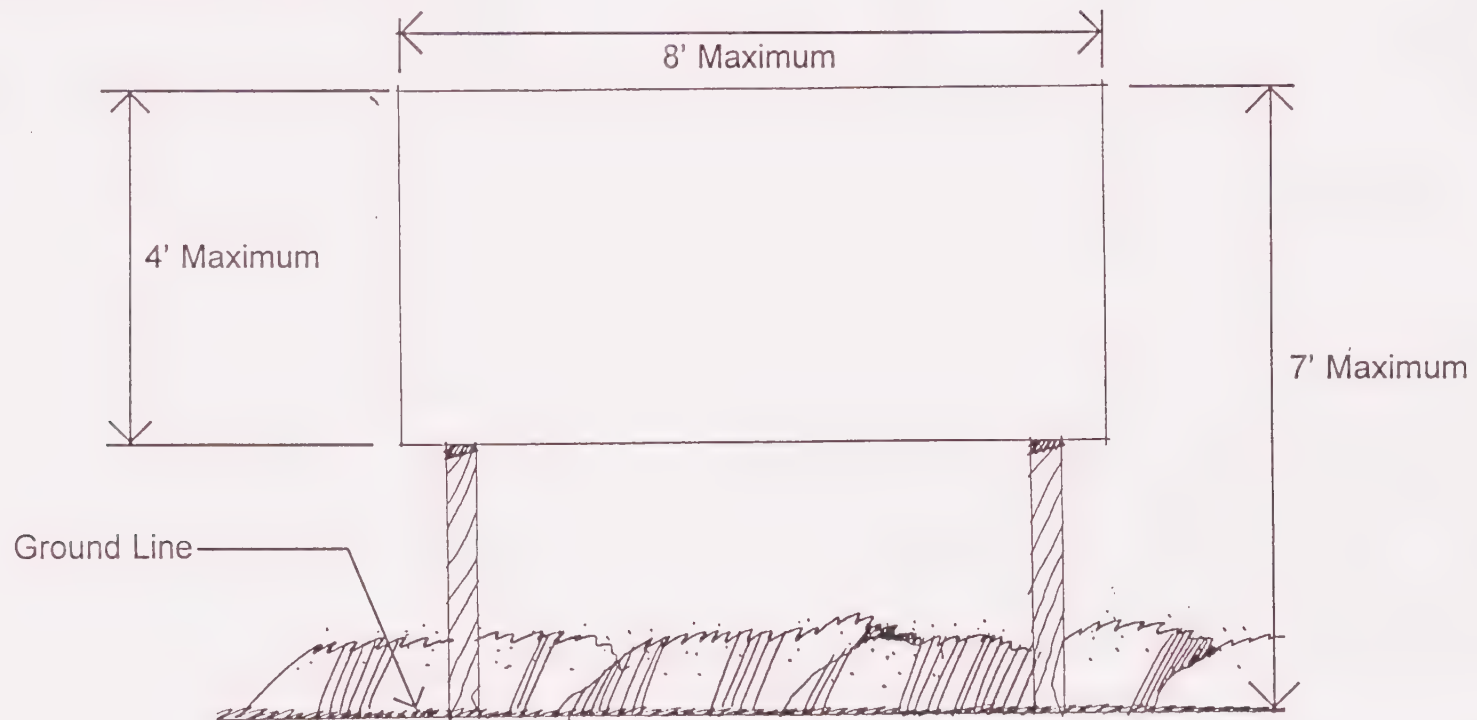
5.4.14 **Informational/directional signs** shall be limited to the identification of function and/or service and shall not contain the name of the business, company or corporation providing the function and/or service. When appropriate, such signs shall contain a directional arrow and provide direction to functions and/or services or information such as Authorized Vehicles Only, Handicapped Parking Only, and Loading Zone.



5.4.15 **Temporary signs** shall be allowed to provide information and facilitate information during the construction and marketing. Such signs shall be limited to one per lot, with a maximum of thirty-two (32) square feet and seven (7) feet in overall height.

5.4.16 **Temporary directory signs** shall be permitted on construction sites, and limited to one (1) for all contractors. The sign shall not exceed thirty-two (32) square feet, unless legally required by government contracts to be larger. The sign shall not exceed seven (7) feet in overall height and shall be located no less than ten (10) feet from any property line. These signs shall be removed upon completion of the project.

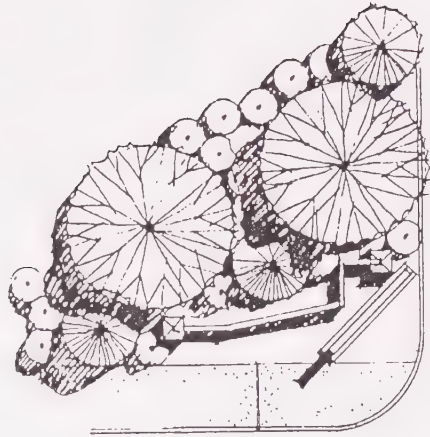
5.4.17 **Future tenant signs** may be placed on vacant or developing property to advertise the future use of the property and where this information may be obtained. Such signs shall be limited to one per street frontage and to a maximum of thirty-two (32) square feet in area and seven (7) feet in overall height. These signs shall be placed no less than ten (10) feet from the property line. Any such sign shall be removed upon occupancy of the project.



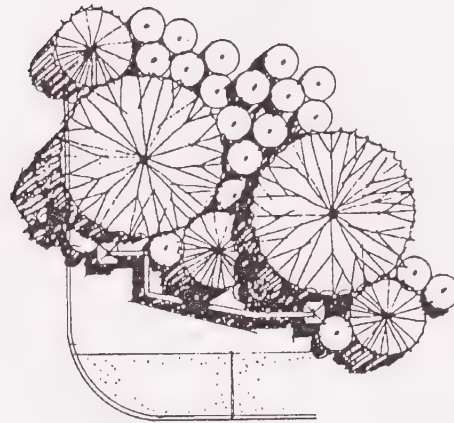
Temporary Sign Detail

Exhibit 48

- 5.4.18 **Centre identification signs** may also be installed at key intersections, in landscaped areas within the right-of-way or landscaped street medians. Centre identification signs shall be limited to thirty-two (32) square feet and not exceed four (4) feet in vertical height, and only identify the overall business park.



Side Entry Sign



Median Entry Sign

Centre Identification Signs

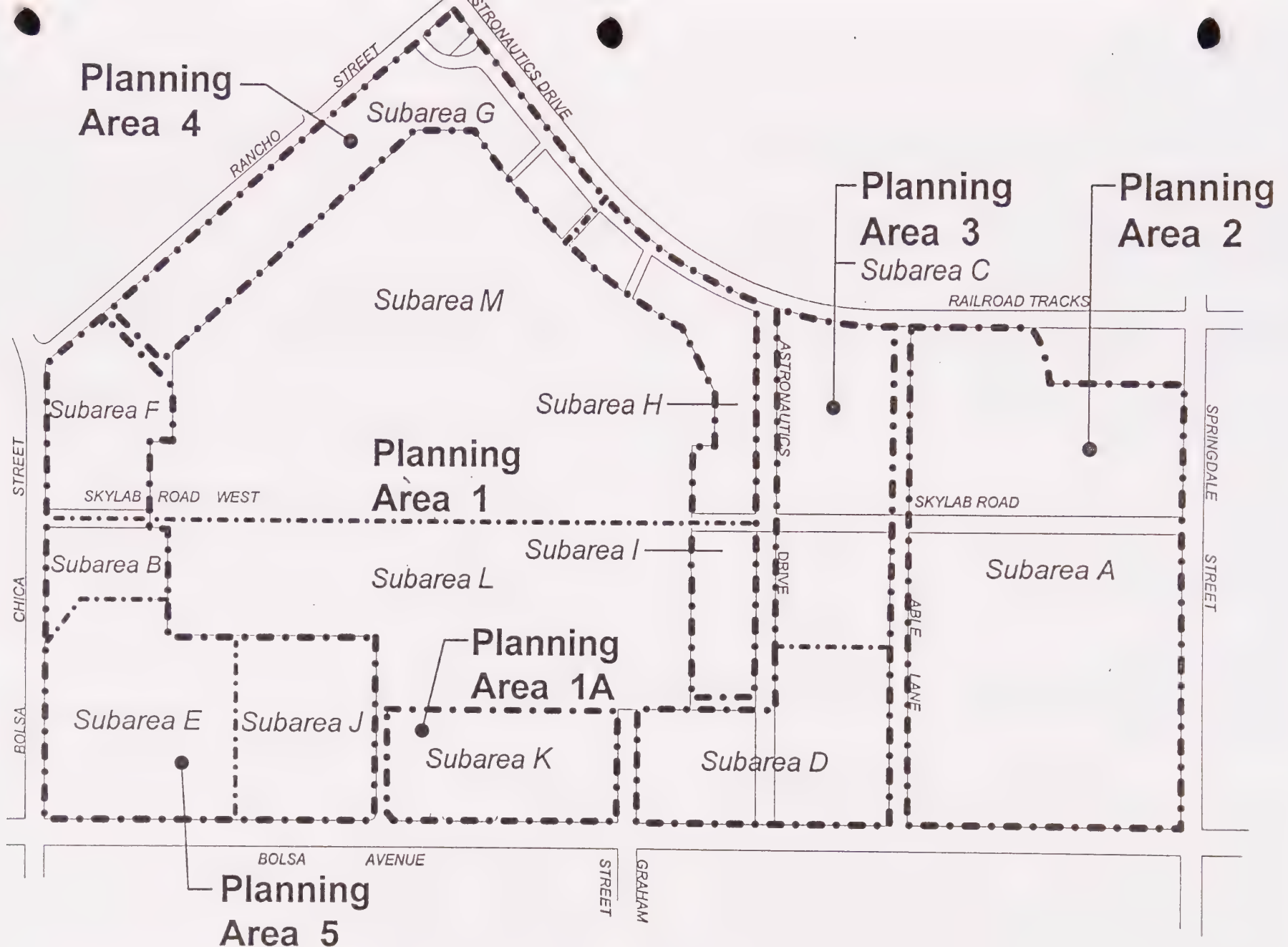
Sign Detail

Exhibit 49



± 585





Planning Areas and Subareas Map

Exhibit 50



3372

DEVELOPMENT REGULATIONS

6.0 PURPOSE

The purpose of this section is to provide specific development regulations and standards that will be applied to individual development projects in each Planning Area of the Specific Plan. Upon adoption by the City of Huntington Beach, the McDonnell Centre Business Park Specific Plan will be the zoning document for the project area.

6.1 GENERAL PROVISIONS

The provisions contained herein shall govern the design and development of the McDonnell Centre Business Park Specific Plan area. Standards and/or criteria for development and activities not specifically addressed in this Specific Plan may require referral to the current provisions of the Huntington Beach Zoning and Subdivision Ordinance and Municipal Code.

Whenever a use has not been specifically listed as being a permitted use in a particular Planning Area of the Specific Plan, it shall be the duty of the Director of Community Development to determine if the use is consistent with the intent of this Specific Plan and compatible with other permitted uses. In the case of any conflicting provisions the regulation and policies of the Specific Plan shall prevail. In addition, all projects must comply with the following policies.

6.1.0 POLICIES:

- 6.1.1 Notwithstanding provisions to the contrary, all grading shall be approved by both the Director of Community Development and Director of Public Works.
- 6.1.2 All construction shall comply with all provisions of the current Uniform Building Code and as subsequently amended in the future and applicable sections of the Huntington Beach Municipal Code.
- 6.1.3 Construction may commence only after the Director of Community Development finds that the project is consistent with the regulations, and applicable policies and guidelines of the Specific Plan.
- 6.1.4 All structures in existence at the time of adoption shall be deemed in conformance with the Specific Plan. All existing buildings and facilities are deemed to be permitted and are in conformity with the Specific Plan.
- 6.1.5 Request for expansion and/or new construction of aerospace buildings and facilities within Planning Area 1 for aerospace related uses shall be compatible with the existing uses and subject to the limited industrial ("IL") & ("IL-H") provisions of the current Huntington Beach Zoning and Subdivision Ordinance and Municipal Code. This type of project shall not be subject to the provisions of the Specific Plan..
- 6.1.6 Expansion of the existing aerospace facility beyond Planning Area 1 into an adjacent Planning Area shall

be subject to both the current provisions of the City's Zoning and Subdivision ordinance and The McDonnell Centre Business Park Specific Plan.

6.2 DEFINITIONS

For the purposes of the Specific Plan, words, phrases and terms shall have the meanings as defined below. Terms not specifically defined in the Specific Plan shall have the same definition as used in the City of Huntington Beach Zoning and Subdivision Ordinance at the time of Specific Plan adoption.

When not inconsistent with the context, words used in the present tense include the future tense; words used in the singular number include the plural number; and words of the masculine gender include the feminine and neuter gender. The word "shall" is always mandatory and the word "may" is permissive.

- 6.2.1 **Accessory building.** A detached building on the same site as a main building, the use of which is incidental to that of the main building such as a storage shed, etc., and which is used exclusively by the occupants of the main building.
- 6.2.2 **Antenna, satellite dish.** An apparatus capable of receiving communications from a transmitter or transmitter relay.
- 6.2.3 **Architectural projections or appurtenances.** Features on buildings which provide visual variation

and/or relief but do not serve as interior or exterior living or working space.

- 6.2.4 **Building height.** The vertical dimension measured from the top of the highest roofline to the finished pad elevation shown on the approved grading plan.
- 6.2.5 **Building, main.** A building in which the principal use of the lot is conducted.
- 6.2.6 **Director.** Director of Community Development for the City.
- 6.2.7 **Entryway.** The point of ingress and egress from a public or private street to the individual project.
- 6.2.8 **Final approval.** Ten (10) days after approval by the discretionary body and no appeal of that decision has been filed.
- 6.2.9 **Grade.** The surface of the ground or pavement at a stated location as it exists prior to disturbance in preparation for a project by this code, includes natural grade.
- 6.2.10 **Gross floor area.** The total area of each floor within the building exterior walls.
- 6.2.11 **Gross site area.** The area computed within the lot lines of a parcel of land before public streets, easements, or other areas, to be dedicated or reserved for public use, have been deducted.



- 6.2.12 **Landscaping.** The planting and continued maintenance of suitable plant materials, and which includes an adequate irrigation system.
- 6.2.13 **Line of sight.** A visual path emanating from an average eye level adjudged to be five (5) feet above ground level.
- 6.2.14 **Local street.** A low-speed, low-volume thoroughfare used primarily for access to individual properties.
- 6.2.15 **Lot.** Any numbered or lettered parcel shown on a recorded tract map, a record of survey pursuant to an approved division of land, or a parcel map. A lot includes any area of land under one ownership abutting upon at least one street, alley or recorded easement.
- 6.2.16 **Lot area.** See net site area.
- 6.2.17 **Lot depth.** The average horizontal distance between the front and rear property lines, measured in the mean direction of the side property lines.
- 6.2.18 **Lot frontage.** The linear length of a lot measured along the property line adjacent to a street or easement.
- 6.2.19 **Lot line.** Any line bounding a lot. "Property line" means the same as "lot line."

- 6.2.20 **Lot line, front.** On an interior lot, the front lot line is the property line abutting the street. On a corner or reverse corner lot, the front lot line is the shorter property line abutting a street, except in those cases where the subdivision or parcel map specified another line as the front lot line. On a through lot, or a lot with three or more sides abutting a street, or a corner or a reverse corner lot with lot lines of equal length, the Director shall determine which property line shall be the front lot line for the purposes of compliance with yard and setback provisions of this division. On a private street or easement, the front and/or exterior lot line shall be designed as the edge of the easement.
- 6.2.21 **Lot line, interior.** A lot line not abutting a street.
- 6.2.22 **Lot line, rear.** A lot line not abutting a street which is opposite and most distant from the front lot line; in the case of an irregularly shaped lot, the rear lot line shall be determined by the Director. A lot which is bounded on all sides by streets may have no rear lot lines.
- 6.2.23 **Lot width.** Lot width shall be calculated as indicated for the following types of lots:
- (a) Rectangular lot shall be measured along a line equidistant to and twenty (20) feet from the front property line.
 - (b) Cul-de-sac and knuckle lots shall be measured twenty (20) feet from the front property line along a line perpendicular to the bisector of the front property line.
 - (c) Cul-de-sac lots siding on another street, or similar properties, shall be measured along a line perpendicular to the interior side property line and twenty (20) feet from the front property line.

- 6.2.24 **Net site area.** The total horizontal area within the property lines of a parcel of land exclusive of all rights-of-way, easements or dedications which physically prohibit the surface use of that portion of the property for other than vehicular ingress and egress, and/or landscaping.
- 6.2.25 **Open space.** Any part of a lot or parcel unobstructed from the ground upward, excepting architectural features extending no more than thirty (30) inches from the structure. Driveways and other parking areas shall not be considered open space..
- 6.2.26 **Parking structure.** A structure used for parking of vehicles where parking spaces, turning radii and drive aisles are incorporated within the structure.
- 6.2.27 **Person.** The word "person" includes association, company, firm, corporation, partnership, copartnership or joint venture.
- 6.2.28 **Private street.** A privately owned and maintained roadway used to provide vehicle access to abutting properties.
- 6.2.29 **Retail commercial use.** Permitted uses within this designation include restaurants, hotels, entertainment and general merchandise establishments.
- 6.2.30 **Setback line.** The line which defines the width or depth of the required yard. Such line shall be parallel to the property line and removed therefrom by the perpendicular distance described as the setback.

- 6.2.31 **Site.** Any legally created parcel of land bounded by property lines after dedication.
- 6.2.32 **Site coverage.** The building area of all structures on a site, as measured from all exterior building surfaces. Architectural features such as bay windows, eaves and canopies that do not project more than thirty (30) inches, and decks that do not exceed more than forty-two (42) inches in height are excluded.
- 6.2.33 **Site plan.** A plan prepared to scale, showing accurate and complete dimensions of all: buildings, structures, landscaping, parking, drive aisles, uses, etc. and the exact manner of development proposed for a specific parcel of land.
- 6.2.34 **Story.** That portion of a building included between the surface of any floor and the surface of the floor next above it or the finished under surface of the roof directly above.
- 6.2.35 **Street.** A public or approved private thoroughfare or road easement which affords the principal means of access to abutting property.
- 6.2.36 **Structure.** Any building or portion thereof, wall, fence, etc., extending forty-two (42) inches in height above the grade.
- 6.2.37 **Structural alteration.** Any change in, or alterations to, the structure of a building involving: the bearing wall, column, beam or ceiling joints, roof rafters, roof diaphragms, foundations, retaining walls or similar components.



- 6.2.38 **Ultimate right-of-way.** The adopted maximum width for any street, alley or thoroughfare as established by: the general plan, a precise plan of street, alley or private street alignment, a recorded parcel map, or a standard plan of the department of Public Works. Such thoroughfares shall include any adjacent public easement used as a walkway and/or utility easement.
- 6.2.39 **Use.** The purpose for which land or building is arranged, designed, or intended, or for which it is occupied or maintained.
- 6.2.40 **Wall or fence.** Any structure or devise forming a physical barrier. This definition shall include: wood, concrete, concrete block, brick, stone or other masonry material.
- 6.2.41 **Yard.** An open, unoccupied space on a lot on which a building is situated and, except where provided in the ordinance code, is completely unobstructed from the ground to the sky.
- 6.2.42 **Yard, front.** A yard extending across the full width of the lot between the side lot lines and between the front lot line and either the nearest line of the main building or the nearest line of any enclosed or covered porch. The front lot line shall be deemed to be the existing nearest right-of-way line of the abutting street, road or highway, unless a different right-of-way line for future use shall have been precisely fixed by formal action of the City Council pursuant to law or ordinance.
- 6.2.43 **Yard, rear.** A yard extending across the full width of the lot between the side lot lines and measured between the rear lot line and the nearest rear line of the main building or the nearest line of any enclosed or covered porch.
- 6.2.44 **Yard, side.** A yard extending from the front yard to the rear yard between the side property line and the nearest line of the main building or any accessory building.
- 6.2.45 **Zone.** A district as defined in the state Conservation and Planning Act, shown on the official zoning maps and to which uniform regulations apply.
- 6.2.46 **Zoning maps.** the official zoning maps of the City of Huntington Beach which are a part of the comprehensive zoning ordinance.

Permitted Uses	Planning Area					
	1	1a	2	3	4	5
INDUSTRIAL						
MDA Aerospace, including all existing buildings and facilities as well as expansion of similar facilities under the existing development standards. Such uses may include tank fabrication and assembly operation, heavy welding, insulation, and thermal protective coatings.	●	●		●	●	●
Manufacturing	●	●	●	●	●	●
Warehousing	●	●	●	●	●	●
Light Industrial	●	●	●	●	●	●
Research and Development	●	●	●	●	●	●
COMMERCIAL						
Banks and other Financial Institutions		●				●
Commercial Recreation and Entertainment						●
Communication Facilities	●	●	●	●	●	●
Eating & Drinking Establishments	●	●	●	●	●	●
Hotels, Motels and Ancillary Retail Uses		●	●	●	●	●
Retail Sales		●		●		
Maintenance & Repair Services	●	●	●	●	●	●
Warehouse & Sales Outlets	●	●	●	●	●	●
OFFICE						
Business & Professional	●	●	●	●	●	●
Personal Services		●				●
Research & Development Services	●	●	●	●	●	●
Laboratories	●	●		●	●	●
PUBLIC AND SEMIPUBLIC						
Conference Facilities	●	●		●		●
Day Care, General	●	●	●	●	●	●
Governmental Facilities	●	●		●	●	●
Heliports Maintenance and Service Facilities	●				●	
Public Utilities and Facilities	●	●	●	●	●	●

Exhibit 51



6.3 DEVELOPMENT STANDARDS

The Development Standards shall serve as the mechanism for the implementation of the McDonnell Centre Business Park land uses. The standards set forth in this section will assure that future development within the Centre is implemented in a manner consistent with the intent of the project area Master Plan. The standards contained herein provide flexible mechanisms to anticipate future needs and achieve compatibility between land uses and the surrounding community. Standards and guidelines are designed to be compatible with the existing land use categories of the City. The primary land uses in the McDonnell Centre Business Park shall be industrial and research and development. Commercial, office, public and semi-public uses are secondary and may be permitted in certain Planning Areas. Development Standards shall be established for each Planning Area in order to accommodate a variety of alternative uses in each area.

- 6.3.1 **Permitted Uses.** Permitted uses shall be established in each Planning Area and shall be required to meet all applicable provisions of the Huntington Beach Ordinance Code.

All structures incidental and accessory to a permitted principal use or structure, may be erected on any parcel containing a main building provided that such structures conform with all requirements of the Specific Plan.

Parking structures may be constructed in each Planning Area and subject to all the development regulations of the Planning Area with the exception of maximum floor area ratio and maximum site coverage. No maximum shall be established for these provisions.

- 6.3.2 **Maximum building height.** The maximum allowable building height shall be established in each Planning Area. An additional ten (10) feet in height will be allowed for roofline treatment, architectural features and special equipment or mechanical devices. Building height may also be increased by fourteen (14) feet to allow roof top mechanical, multi story buildings, (three (3) or more) may request up to twenty (20) percent of the building height for mechanical equipment housing.

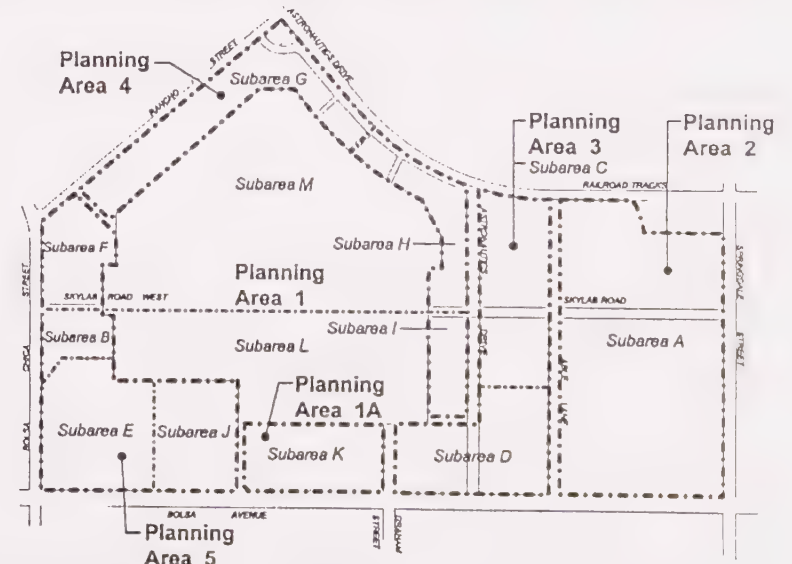
- 6.3.3 **Intensity.** Maximum intensity shall be established in each Planning Area. The overall intensity for the Specific Plan area has been established by the City's General Plan with a floor area ratio (F.A.R. of .75.) The maximum F.A.R. for each Planning Area varies to accommodate the anticipated uses.

Development Regulations Chart

	1	1		1A	2	3		4			5			
		L	M	K	A	C	D	G	H	I	B	E	F	J
Maximum Intensity (F.A.R.)	0.75	0.75	0.65	0.75	0.65	0.65	0.75	0.65	0.65	0.65	0.75	0.75	0.75	0.75
Minimum Lot Size (AC)	NA	2.5	2.5	1.0	2.5	2.5	2.5	1.0	2.5	2.5	1.0	1.0	1.0	1.0
Minimum Lot Frontage	100'	250'	250'	250'	250'	250'	250'	250'	250'	250'	250'	250'	250'	250'
Maximum Bldg. Height	250'	50'	50'	50'	50'	50'	50'	40'	40'	50'	50'	175'	75'	75'
Maximum Lot Coverage	50%	60%	60%	60%	60%	60%	60%	60%	60%	60%	60%	60%	60%	60%
Minimum Setback														
	Front	20'	25'	35'	35'	35'	35'	25'	25'	25'	35'	35'	35'	35'
	Interior Side	15'	10'	15'	10'	15'	15'	10'	10'	15'	15'	10'	10'	10'
	Exterior Side	10'	15'	25'	15'	25'	25'	15'	15'	25'	25'	15'	15'	15'
	Rear		10'	15'	10'	15'*	15'*	10'	10'	15'	15'	10'	10'	10'
Minimum Landscape	8%	10%	10%	15%	10%	10%	15%	10%	10%	10%	15%	15%	15%	15%
Minimum Perimeter Landscape														
	Front		15'	15'	15'	15'	15'	15'	10'	15'	15'	15'	15'	15'
	Interior Side		5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'
	Exterior Side		10'	10'	10'	10'	10'	10'	10'	10'	10'	10'	10'	10'
	Interior Rear		5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'	5'
	Exterior Side		10'	10'	10'	10'	10'	10'	10'	10'	10'	10'	10'	10'
Adjacent to Arterial Hwy.		NA	NA	24'	24'	NA	24'	24'	NA	NA	24'	24'	24'	24'

* Minimum rear setback 50' when adjacent to residential areas.

Exhibit 52



6.3.4 Parking. All developments will be required to meet the minimum on-site parking standards of the Huntington Beach Zoning and Subdivision Ordinance. In addition, the following may apply:

- Standard parking stall size shall be nine (9) feet wide by nineteen (19) feet deep and may be reduced to provide a wheel stop at seventeen (17) feet with a two (2) foot overhang to expand the landscaping. This additional landscape area will not be credited toward the required landscape percentage.
- A compact parking stall size of eight feet zero inches (8'-0") wide by seventeen (17) feet deep may be proposed for employee parking areas, up to a maximum of twenty five (25) percent of the total required parking spaces.
- Total parking required by the Huntington Beach Zoning and Subdivision Ordinance may be installed in phases, as needed, provided sufficient parking for employee projections can be met. The initial phase of required parking shall be a minimum of fifty (50) percent of the required parking. A covenant shall be recorded on the property prior to occupancy to insure provision of all parking as necessary.
- Shared parking may be permitted for combined office and commercial uses. A shared parking program may allow for a reduction of code required parking by up to twenty five (25) percent, based upon a shared parking analysis.

Parking Standards

Use	Required Parking
Office Administrative Research and Development	1 sp./250 s.f.
Industrial and Manufacturing	1 sp./500 s.f.
Warehouse and Distribution	1 sp./500 s.f. (for the first 20,000 s.f.) 1 sp./5,000 s.f. (for over 20,000 s.f.)
Hotel/Motel	.75 sp./room plus 2 sp./Manager
Restaurant	1 sp./100 s.f.
Retail	1 sp./200 s.f.

Exhibit 53

- Shared off-site parking facilities may also be permitted for adjacent office and industrial uses.
- Handicap accessible parking spaces shall be provided as required by the California Building Code.
- All areas set aside for future parking facilities shall be landscaped consistent with other on-site landscaping and may not be used for building development or expansion.
- Any existing aerospace required surface parking as described in the Specific Plan parking requirements, removed for new development shall be replaced on a one for one basis concurrent with new development.

6.3.5 **Landscaping.** In addition to the Landscape Guidelines contained in the Specific Plan and City Standard Landscape Plans: all setback and parking lot areas fronting on, or visible from, adjacent public streets shall be landscaped and permanently maintained in an attractive manner.

6.3.6 **Signs.** All signs in the project area shall conform to the provisions of the City's Zoning and Subdivision Ordinance and shall be consistent with the guidelines of the Specific plan.

6.3.7 **Lighting.** All illumination of streets, parking areas, and project sites, shall be coordinated to provide a consistent illumination intensity. Emphasis shall be placed on areas of high vehicular and pedestrian activity. Light fixtures and standards shall be consistent with building architectural style.

6.3.8 **Fencing.** Walls and fences are encouraged as means of providing security and screening. Walls shall be constructed of masonry or concrete materials consistent with, and complimentary to, building architecture. Fencing shall be restricted to ornamental iron; chain-link fencing shall not be used.



Development Regulations Check List

	Required/ Allowed	Request	Comments
Use			
Intensity (F.A.R.)			
Lot Size (AC)			
Lot Frontage			
Building Height			
Lot Coverage			
Setback			
Front			
Interior Side			
Exterior Side			
Rear			
Landscape			
Perimeter Landscape			
Front			
Interior Side			
Exterior Side			
Interior Rear			
Exterior Side			
Arterial Highway			

Exhibit 54

Design Guidelines Policy Check List

5.1	Site Planning Guidelines	Good	Modify/ Explain	Redesign	Comments
5.1.1	Site Layout				
5.1.2	Building Orientation				
5.1.3	Parking Layout				
5.1.4	Loading & Storage Layout				
5.1.5	Site Access				
5.1.6	On-Site Circulation				
5.1.7	Entry Drive Design				
5.1.8	Handicap Accessibility				
5.1.9	Energy Conservation				
5.1.10	Security Provisions				
5.1.11	Relationship to Adj Sites				

Exhibit 55



Design Guidelines Policy Check List

5.2 Architectural Guidelines	Good	Modify/ Explain	Redesign	Comments
All Buildings				
5.2.1 Building Location				
5.2.2 Building Massing				
5.2.3 Building Articulation				
5.2.4 Building Relationships				
5.2.5 Building Entries				
5.2.6 Building Materials				
5.2.7 Glazing				
5.2.8 Equipment Screening				
5.2.9 Exterior Lighting				
5.2.10 Trash Enclosures				
Industrial Buildings				
5.2.11 Building Design				
5.2.12 Building Entries				
5.2.13 Building Materials				
5.2.14 Mechanical Equipment				
5.2.15 Satellite Structures				
5.2.16 Loading Areas				
5.2.17 Outdoor Storage				
Commercial/Office Buildings				
5.2.18 Building Design				
5.2.19 Building Materials				
5.2.20 Building Illumination				

Design Guidelines Policy Check List

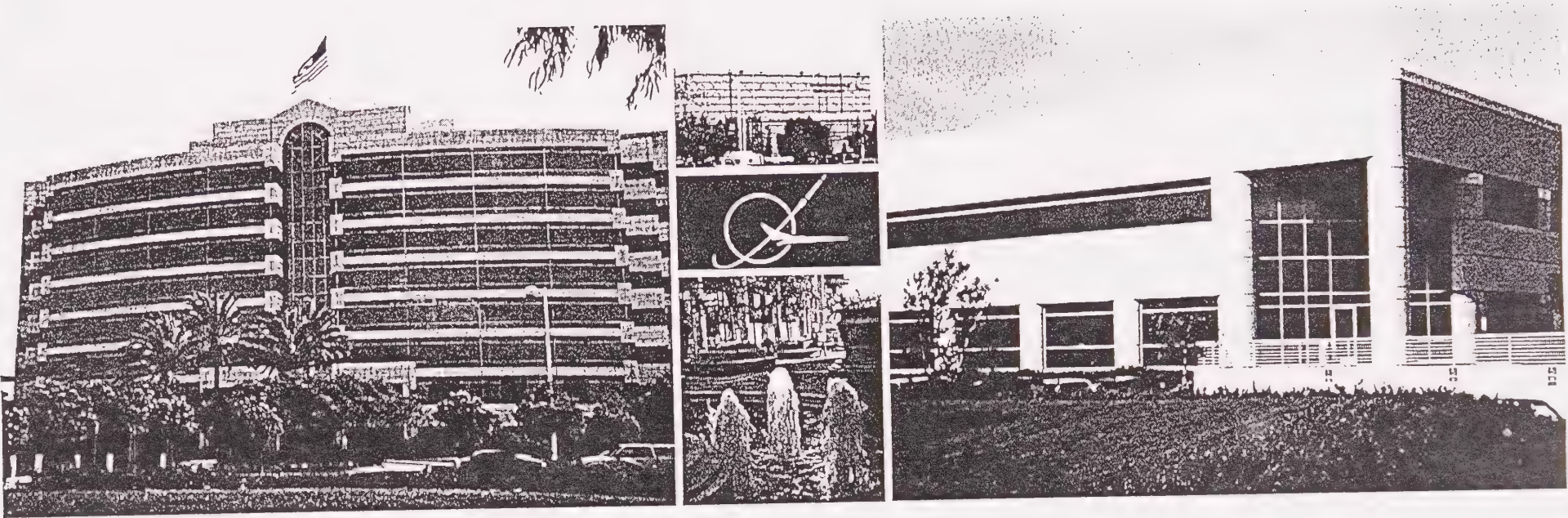
5.3 Landscape Guidelines	Good	Modify/ Explain	Redesign	Comments
5.3.1 Site Layout				
5.3.2 Existing Trees				
5.3.3 Landscape Design				
5.3.4 Plant Materials				
5.3.5 Right-Of-Way Areas				
5.3.6 Pedestrian Walkways				
5.3.7 Perimeter Landscaping				
5.3.8 Parking Lots				
5.3.9 Interior Property Lines				
5.3.10 Entry Drives				
5.3.11 Entry Plazas				
5.3.12 Buffer Areas				
5.3.13 Screening				
5.3.14 Lighting				
5.3.15 Water Conservation				

Exhibit 55



Design Guidelines Policy Check List

5.4	Signage Guidelines	Good	Modify/ Explain	Redesign	Comments
5.4.1	Wall Sign Design				
5.4.2	Wall Sign Location				
5.4.3	Wall Sign Materials				
5.4.4	Wall Sign Size				
5.4.5	Tenant Sign Location				
5.4.6	Tenant Sign Quantity				
5.4.7	Additional Wall Sign Design/Size				
5.4.8	Free Standing Sign Design/Size				
5.4.9	Entry Sign Design/Size				
5.4.10	Entry Sign Content				
5.4.11	Business Directory Sign Content				
5.4.12	Business Directory Sign Design/Size				
5.4.13	Information/Directional Sign Design/Size				
5.4.14	Information/Directional Sign Content				
5.4.15	Temporary Sign Design/Size				
5.4.16	Temporary Directory Sign Design/Size				
5.4.17	Future Tenant Sign Design/Size				
5.4.18	Centre Identification Sign Design/Size				



McDONNELL CENTRE BUSINESS PARK

City of Huntington Beach

SPECIFIC PLAN NO. 11

(VOLUME TWO)

McDONNELL CENTRE BUSINESS PARK

City of Huntington Beach

SPECIFIC PLAN NO. 11

APPENDICES
(VOLUME TWO)



Prepared by
EDAW, Inc. with
Michael C. Adams Associates

TABLE OF CONTENTS

	<u>Page</u>
A. LEGAL DESCRIPTION	1
B. GENERAL PLAN CONSISTENCY ANALYSIS	3
1. Land Use Element	4
2. Urban Design Element	13
3. Housing Element	14
4. Historic and Cultural Resources Element	14
5. Economic Development Element	15
6. Growth Management Element	16
7. Circulation Element	22
8. Public Facilities and Public Services Element	25
9. Recreation and Community Services Element	26
10. Utilities Element	27
11. Environmental Resources/ Conservation Element	28
12. Air Quality Element	30
13. Coastal Element	33
14. Environmental Hazards Element	33
15. Noise Element	35
16. Hazardous Materials Element	37
C. ENVIRONMENTAL MITIGATION MEASURES	37

A. LEGAL DESCRIPTION

The following provides the legal description for the entire McDonnell Centre Business Park Specific Plan area, which is bounded by an at grade spurtrack of the US Navy (railroad right-of-way) and Rancho Road on the north, Springdale Street on the east, Bolsa Avenue on the south and Bolsa Chica Street and the Orange County Flood Control District channel on the west. Refer to Exhibit 1 in the Specific Plan.

A portion of fractional Section 9, Township 5 South, Range 11 West, partly in the Rancho Las Bolsas and partly in the Rancho La Bolsa Chica, in the City of Huntington Beach, County of Orange, State of California, as per map recorded in Book 51, page 13 of Miscellaneous Maps, in the Office of the County Recorder of said county, described as follows:

Commencing at the Southeast Corner of said section, said point being also the center line intersection of Springdale Street and Bolsa Avenue as shown on a map of Parcel Map No. 93-202, recorded in Book 281, pages 3 and 4 of Parcel Maps, in the Office of said County Recorder; thence North $0^{\circ}16'04''$ East 2055.80 feet along the center line of Springdale Street, said center line being also the easterly line of said section; thence North $89^{\circ}28'58''$ West 50.00 feet to a point on the westerly line of Springdale Street as shown on said parcel map, said point being also the northeast corner of Parcel 3 of said parcel map and the Point of Beginning; thence, along said westerly line and parallel with the easterly line of said section, South $0^{\circ}16'04''$ West 1965.58 feet to the southerly terminus of the easterly line of Parcel 1 of Parcel Map No. 95-136 as shown on a map thereof recorded in Book 288, pages 49 and 50 of Parcel Maps; thence, along a tangent 30.00 foot radius curve, concave northwesterly, through a central angle of $90^{\circ}18'36''$, a distance 47.29 feet to a tangent line which is parallel with and 60.00 feet

northerly of the southerly line of said section, said last mentioned southerly line being also the center line of Bolsa Avenue as shown said last mentioned parcel map, said tangent line being also the southerly line of said Parcel 1; thence along said tangent line, North $89^{\circ}25'20''$ West 23.70 feet to tangent 38.00 foot radius curve, concave northerly; thence, continuing along the southerly line of said Parcel 1 and said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 5.80 feet; thence, tangent to said curve, North $80^{\circ}40'34''$ West 58.12 feet to the beginning of a tangent 62.00 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 9.46 feet; thence, tangent to said curve, North $89^{\circ}25'20''$ West 52.35 feet to the beginning of a tangent 62.00 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 9.46 feet; thence tangent to said curve, South $81^{\circ}49'54''$ West 58.12 feet to a tangent 38.00 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $8^{\circ}44'46''$, a distance of 5.80 feet to a tangent line which is parallel with and 60.00 feet northerly of the southerly line of said section; thence, along said last mentioned tangent line, North $89^{\circ}25'20''$ West 4117.58 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23 feet; thence, tangent to said curve, North $72^{\circ}43'24''$ West 20.88 feet to the beginning of a tangent 83.13 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence, tangent to said curve, North $89^{\circ}25'20''$ West 49.12 feet to the beginning of a tangent 83.13 foot radius curve, concave southerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 24.23 feet; thence, tangent to said curve, South $73^{\circ}52'45''$ West 20.88 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of $16^{\circ}41'56''$, a distance of 17.23

feet to a tangent line which is parallel with and 60.00 feet northerly of the southerly line of said section; thence, along said last mentioned tangent line, North 89°25'20" West 597.93 feet to the westerly terminus of the southerly line of Parcel 2 of Parcel Map No. 87-424, as shown on a map thereof recorded in Book 237, pages 4 through 6, inclusive, of Parcel Maps in the Office of said County Recorder; thence, along a tangent 30.00 foot radius curve, concave northeasterly, through a central angle of 90°02'30", a distance of 47.15 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map, said tangent line being also the westerly line of said Parcel 2; thence, along said tangent line, North 0°37'10" East 221.35 feet to a tangent 180.75 foot radius curve, concave easterly; thence, continuing along the westerly line of said parcel and along said tangent curve, through a central angle of 9°26'16", a distance of 29.77 feet; thence, tangent to said curve, North 10°03'26" East 31.14 feet to a tangent 180.75 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of 9°26'16", a distance of 29.77 feet; thence, tangent to said curve, North 10°03'26" East 227.87 feet to the beginning of a non-tangent 18.00 foot radius curve, concave northeasterly, a radial to the beginning of said curve bears South 27°00'26" West; thence, along said non-tangent curve, through a central angle of 63°36'44", a distance of 19.98 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map; thence, along said tangent line, North 0°37'10" East 49.56 feet to a tangent 59.13 foot radius curve, concave easterly; thence, along said tangent curve, through a central angle of 16°41'56", a distance of 17.23 feet; thence, tangent to said curve, North 17°19'06" East 20.88 feet to the beginning of a tangent 83.13 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of 16°41'56", a distance of 24.23 feet; thence, tangent to said curve, North 0°37'10" East 49.12 feet to the beginning of a tan-

gent 83.13 foot radius curve, concave westerly; thence, along said tangent curve, through a central angle of 16°41'56", a distance of 24.23 feet; thence tangent to said curve, North 16°04'45" West 20.88 feet to a tangent 59.13 foot radius curve, concave northerly; thence, along said tangent curve, through a central angle of 16°41'56", a distance of 17.23 feet to a tangent line which is parallel with and 80.00 feet easterly of the center line of Bolsa Chica Road as shown on said last mentioned parcel map; thence, along said tangent line, North 0°37'10" East 2,031.46 feet to a tangent 30.00 foot radius curve, concave southeasterly; thence, along said tangent curve, through a central angle of 49°52'31", 26.11 feet to a tangent line which is parallel with and 50.00 feet southeasterly of the center line of Rancho Avenue as shown on a Record of Survey recorded in Book 60, page 13 of Record of Surveys; thence, along said tangent line, North 50°29'41" East 2382.14 feet to the southerly line of the 75.00 foot wide Right-of-Way described in deed recorded in Book 1296, page 88 of Deeds in the Office of the County Recorder of said county; thence, along said southerly line, South 39°28'30" East 877.72 feet to a tangent 1,947.78 foot radius curve, concave northeasterly; thence, along said tangent curve and said southerly line, through a central angle of 50°00'28", 1,700.02 feet to a tangent line, said tangent line being also the westerly prolongation of the northerly line of Parcel 1 of said previously mentioned Parcel Map No. 93-202; thence, along said tangent line, South 89°28'58" East 492.00 feet to the northeasterly corner of said last mentioned Parcel 1 and the beginning of a non-tangent 383.39 foot radius curve, concave southeasterly, a radial to the beginning of said curve bears North 32°30'55" East; thence, along the northeasterly line of said parcel and said non-tangent curve, through a central angle of 47°51'40", 320.26 feet to the northwesterly corner of Parcel 2 of said Parcel Map No. 93-202; thence, along the northerly line of Parcel 2 and Parcel 3 of said parcel map, South 89°28'58" East 598.07 feet to the Point of Beginning.
Containing 308.44 acres, more or less.

B. GENERAL PLAN CONSISTENCY ANALYSIS

California State law requires that all cities and counties have a long range general plan for their physical development. Once a local government has adopted its General Plan, it must be implemented, and local governments have a range of implementation tools to select from. Most mechanisms for implementing a City's general plan derive from local government's corporate and police powers, such as: construction of streets, acquisition and development of parks, zoning, subdivision regulations, school dedication requirements, code enforcement, environmental and design review procedures and redevelopment.

A specific plan is an effective implementation tool that is often used to address a single project or a master planned project such as the McDonnell Centre Business Park. The Specific Plan provides a bridge between the broad General Plan policies and individual project submittal (site plans, subdivision proposals, etc.) in a more area-specific manner than is possible with community wide zoning ordinances. As a result, a specific plan's emphasis focuses on establishing guidelines and concrete development standards to supplement those of the general plan. Specific plans must be consistent with all facets of the general plan, including the policy statements contained within the general plan document. In turn, zoning, subdivisions, and public works projects must be consistent with the Specific plan.

The McDonnell Centre Business Park Specific Plan contains detailed regulations, guidelines and implementation measures that will serve as a guide, providing consistency with the City's general plan and standards and guidelines by which future development will be approved.

This section explains how the McDonnell Centre Business Park Specific Plan achieves consistency with the City of Huntington Beach General Plan. The four (4) points listed below provide the basis for the consistency analysis contained in this section.

1. California Government Code Section 65450-65553 permits adoption and administration of Specific Plans as an implementing tool for the General Plan.
2. The current General Plan for the site is "Industrial" with a Floor Area Ratio (F.A.R.) of 0.75. The site is currently zoned "IL" Limited Industrial and "IL-H" Limited Industrial - High Rise Overlay. There is no restriction in the zoning code concerning Floor Area Ratios. The adoption of the Specific Plan will supersede the existing zoning and adopt a new set of zoning regulations. Adoption of the Specific Plan is consistent with the General Plan; therefore, a General Plan amendment is not required.
3. The Specific Plan must be consistent with the General Plan and serve to implement all aspects of the General Plan as it applies to the designated area.
4. The City of Huntington Beach's General Plan Update, adopted in 1995, is comprised of 16 separate elements: 1) land use, 2) urban design, 3) housing, 4) historic and cultural resources, 5) economic development, 6) growth management, 7) circulation, 8) public facilities and public services, 9) recreation and community services, 10) utilities, 11) environmental resources/ conservation, 12) air quality, 13) environmental hazards, 14) noise, 15) coastal and 16) haz-

ardous materials. The following provides a brief discussion of these Elements which are applicable to the project including a listing of applicable goals and policies. Although the General Plan identifies objectives within each element, the policies actually implement the objectives and therefore provide more specific criteria of how the identified goals will be achieved. The discussion below indicates how the Specific Plan project meets the applicable policies.

1. LAND USE ELEMENT

The Land Use Element (LUE) for the City of Huntington Beach General Plan provides for the types, density/intensity, design, and distribution of commercial, residential, industrial, and agricultural land uses as well as public and private open space. The LUE includes goals designed to serve as a general guide for the future development of Huntington Beach in terms of location of uses, allowable residential densities, and other criteria.

The LUE designates the 307-acre McDonnell Centre Business Park project site Industrial with an FAR of 0.75. Typical permitted uses of the Industrial designation are light manufacturing, research and development, warehousing, business parks and professional offices, supporting retail, financial, and restaurants, and similar uses, or warehouse and sales outlets.

The primary goal of the Land Use Element is to provide guidance regarding the manner in which lands are to be used in the City of Huntington Beach. Applicable goals include:

- Achieve development that maintains or improves the City's fiscal viability and reflects economic demands while maintaining and improving the quality of life for the current and future residents of Huntington Beach.

- Ensure that development is adequately served by transportation infrastructure, utility infrastructure, and public services.
- Achieve and maintain a high quality of architecture, landscape, and public open spaces in the City.
- Ensure that significant environmental habitats and resources are maintained.
- Achieve a diversity of land uses that sustain the City's economic viability, while maintaining the City's environmental resources and scale and character.
- Achieve a pattern of land uses that preserves, enhances, and establishes a distinct identity for the City's neighborhoods, corridors, and centers.
- Achieve the development of industrial uses that provide job opportunities for existing and future residents, as well as the surrounding subregion, and generate revenue for the City.

The following applicable Land Use Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Correlation of Land Use Development with Market Demands

Policies

LU 1.1.1

Establish incentives for the development of uses to support the needs and reflect the economic demands of City residents and visitors.

The Specific Plan area will be incrementally developed in phases over an extended period of time as outlined in Section 4.5 of the document. The project site has been divided into a number

of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

During the formulation of the Specific Plan, staff identified uses to be permitted and prohibited within the document. "Future permitted uses" have the incentive of a "fast track" entitlement process since their approval shall be determined via the Director of Community Development vs. the Planning Commission or City Council.

LU 1.1.2

Promote development in accordance with the Economic Development Element.

The Specific Plan will encourage future development by promoting a comprehensive planned Industrial Park and allowing for future "fast-track" entitlement. This Specific Plan will promote development in accordance with the Economic Development Element. Additionally, the landowner is promoting development through its internal marketing strategies and real estate brokers.

Correlation of Land Use Development with Supporting Public Infrastructure and Services

Policies

LU 2.1.1

Plan and construct public infrastructure and service improvements as demand necessitates to support the land uses specified in the Land Use Plan (as defined in the Circulation and Public Utilities and Services Elements of the General Plan).

The Specific Plan area will be incrementally developed in phases over an extended period of time. The project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Circulation Plan and Public Facilities Plan for water, wastewater and storm drainage have been prepared for the project consistent with all standards and requirements of the applicable service agencies (refer to Sections 4.2 and 4.3 of the Specific Plan). These plans will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measures are provided as Appendix C.

LU 2.1.2

Require that the type, amount, and location of development be correlated with the provision of adequate supporting infrastructure and services (as defined in the Circulation and Public Utilities and Service Elements).

Development will be correlated with infrastructure needs. Section 4.0 of the Specific Plan presents the realistic development proposed for the planning areas and the circulation, public facilities and infrastructure plans which will support the Land Use Plan and reinforce the design concept. In addition, a development phasing plan has been prepared as Section 4.5 of the Specific Plan. The phasing plan identifies the amount and timing of infrastructure required to support various phases of development.

LU 2.1.3

Limit the type, location, and/or timing of development where there is inadequate public infrastructure and/or services to support land use development.

Development will be correlated with infrastructure needs. Section 4.0 of the Specific Plan presents the realistic development proposed for the planning areas and the circulation, public facilities and infrastructure plans which will support the Land Use Plan and reinforce the design concept. In addition, a development phasing plan has been prepared as Section 4.5 of the Specific Plan. The phasing plan identifies the amount and timing of infrastructure required to support various phases of development.

Quality of the City's Built Environment

Policies

LU 4.1.1

Require adherence to or consideration of the policies prescribed for Design and Development in this Plan, as appropriate.

Design Guidelines and Development Regulations are included as Sections 5.0 and 6.0 of the Specific Plan. Section 5.0 establishes a comprehensive set of design guidelines for the entire project area and for individual project development, while Section 6.0 presents a detailed description of the development regulations and standards which are necessary to guide and control new development and carry out the goals and policies of the Specific Plan and the City's General Plan..

LU 4.1.2

Require that an appropriate landscape plan be submitted and implemented for development projects subject to discretionary review.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Sections 5.3 and 4.4 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

LU 4.1.3

Require property owners to maintain landscaping, remove and abate weeds, and replace unhealthy or dead landscape.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Sections 5.3 and 4.4 of

the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

LU 4.1.4

Encourage developers to incorporate mature and specimen trees and other significant vegetation, as defined by the City, that may exist on a site into the design of a development project for that site.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan. The Landscape Plan (Section 4.4) also requires incorporation of mature and specimen trees that may exist on a site into the design.

LU 4.1.5

Consider creating incentives for the use of drought-tolerant species in landscape design.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan. Incentives for the use of drought-tolerant species in landscape design are also addressed in Section 4.4 of the Specific Plan and will be reviewed with the plans for development of individual parcels.

LU 4.1.6

Require that commercial and industrial development incorporate adequate drought-conscious irrigation systems and maintain the health of the landscape.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

The incorporation of adequate drought-conscious irrigation systems and maintenance of the health of the landscape is also addressed in Section 4.4 of the Specific Plan and will be reviewed with the plans for development of individual parcels.

LU 4.1.7

Require that all commercial and industrial landscape be adequately irrigated with automatic irrigation systems.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

The incorporation of automatic irrigation systems is also addressed in Section 4.4 of the Specific Plan and will be reviewed with the plans for development of individual parcels.

LU 4.1.8

Use reclaimed water for the irrigation of public and private landscape, as feasible.

The City does not currently have a reclaimed water supply system available. As indicated in Section 4.4 of the Specific Plan, reclaimed water shall be utilized where and whenever feasible and shall comply with the City's "Water Efficient Landscape Requirements" (Ordinance #1452). Usage will be addressed with future individual requests for development if such a system is available at that time.

LU 4.2.1

Require that all structures be constructed in accordance with the requirements of the City's building and other pertinent codes and regulations; including new, adaptively re-used, and renovated buildings.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications related to new, adaptively reused, and renovated buildings not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measures are provided as Appendix C.

LU 4.2.4

Require that all development be designed to provide adequate space for access, parking, supporting functions, open space, and other pertinent elements.

Section 5.0 Design Guidelines and Section 6.0 Development Regulations of the Specific Plan include requirements for parking, access requirements, supporting functions, open space, etc.

LU 4.2.5

Require that all commercial, industrial, and public development incorporate appropriate design elements to facilitate access and use as required by State and Federal Laws such as the Americans with Disabilities Act.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request. Appropriate design elements to facilitate access and use shall be incorporated in accordance with State and Federal Laws (refer to Section 5.0 Design Guidelines of the Specific Plan).

LU 4.2.6

Monitor the conditions of buildings in the City and enforce pertinent building, municipal and zoning codes to ensure their maintenance and quality.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall super-

sede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Types and Densities of Land Use to be Permitted

Policies

LU 7.1.1

Accommodate existing uses and new development in accordance with the Land Use and Density Schedules (Table LU-2 - see below).

Table LU-2
Land Use Density and Intensity Schedule

COMMERCIAL AND INDUSTRIAL

Commercial and industrial intensities indicate the maximum floor area ratio (FAR) which may be permitted on a site. The actual development intensity may be reduced to account for site conditions and constraints. FAR represents the total building area (floor space, excluding basements, balconies, and stair bulkheads) on a lot divided by the total area of that lot. (Note: commercial FAR's exceeding 0.4 normally necessitate subterranean or subterranean parking to provide adequate space to meet code required parking).

-F2A

Maximum floor area ratio of 0.75.

The Floor Area Ratio of the Planning Areas within the Specific Plan are at or below the 0.75 FAR. Overall, the maximum permitted FAR is 0.67 based on 8,376,366 SF on 289 net acres.

LU 7.1.5

Accommodate the development of a balance of land uses that maintain the City's fiscal viability and integrity of environmental resources.

The Specific Plan area will accommodate the development of a balance of land uses that maintain the City's fiscal viability and integrity of environmental resources. The proposed permitted land uses are consistent with the City's General Plan for the site and the project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

LU 7.1.6

Accommodate the development of additional jobs-generating land uses that improve the 1992 jobs to housing ratio of 0.82 to 1.0 or greater; to meet objectives of the Regional Comprehensive Plan (Southern California Association of Governments) and Air Quality Management Plan. These should capitalize upon existing industrial strengths and emphasizing the clustering of similar or complementary industries.

The Specific Plan permits job-generating land uses that will assist in improving the 1992 jobs to housing ratio. Additionally,

the Specific Plan area will be incrementally developed in phases over an extended period of time as outlined in Section 4.5 of the document. The project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

Additionally, during the formulation of the Specific Plan, staff identified uses to be permitted and prohibited within the document. "Future permitted uses" (which included mainly industrial and manufacturing uses) have the incentive of a "fast track" entitlement process since their approval shall be determined via the Director of Community Development vs. the Planning Commission or City Council.

Distribution and Pattern of Development

Policies

LU 8.1.1

Accommodate land use development in accordance with the patterns and distribution of use and density depicted on the Land Use Plan Map, and in accordance with the principles discussed below.

- a. Enhance a network of interrelated activity centers and corridors by their distinct functional role, activity, and/or form and scale of development.

- e. Improve industrial districts to accommodate the changing characteristics and needs of manufacturing and other industrial sectors.

- f. Intermix uses and densities in large-scale development projects.

The Specific Plan area will accommodate the development of a balance of land uses consistent with the patterns and distribution of use and density depicted on the Land Use Plan Map. The proposed permitted land uses are consistent with the City's General Plan for the site and the project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

Industrial (I)

Permitted Uses

Policies

LU 12.1.1

Accommodate the continuation of existing and development of new manufacturing, research and development, professional offices, supporting retail commercial (including, but not limited to, sales areas for manufacturers and photocopy stores), restaurants, financial institutions, and similar uses in areas designated on the Land Use Plan Map in accordance with Policy 7.1.1.

The McDonnell Centre Business Park Specific Plan development concept outlined in Section 4.0 of the Specific Plan provides for a planned Industrial/Business Park complex in the northwestern portion of the City of Huntington Beach, consistent with the goals and policies of the Huntington Beach General Plan.

The project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

During the formulation of the Specific Plan, staff also identified uses to be permitted and prohibited within the document. "Future permitted uses" have the incentive of a "fast track" entitlement process since their approval shall be determined via the Director of Community Development vs. the Planning Commission or City Council.

LU 12.1.2

Allow for the development of warehouse and sales outlets on industrial sites that are located on and front designated arterial highways in proximity to arterial intersections provided that the use will not adversely impact the economic integrity or cohesiveness of the industrial district in which it is located or substantially reduce job opportunities for the City's residents.

Section 4.0 of the Specific Plan permits these uses in a manner that is consistent with this policy.

LU 12.1.3

Establish incentives for the inclusion of day-care, public meeting rooms, and other community-oriented facilities in industrial districts; possibly including the use of bonus densities, expedited entitlements, or other techniques.

Day-care, conference facilities and other public and semi-public uses are permitted (refer to Section 6.0 of the Specific Plan). As stated previously, during the formulation of the Specific Plan, staff identified uses to be permitted and prohibited within the document. "Future permitted uses" have the incentive of a "fast track" entitlement process since their approval shall be determined via the Director of Community Development vs. the Planning Commission or City Council.

Design and Development

Policies

LU 12.1.4

Require that new and recycled industrial projects be designed and developed to achieve a high level of quality, distinctive character, and be compatible with existing uses.

The McDonnell Centre Business Park Specific Plan provides the framework and guidelines necessary to create a unique high quality industrial, research and development business park complex. The Specific Plan is designed to allow for development in a manner that is compatible with the surrounding neighborhood and City of Huntington Beach (refer to Sections 4.0 and 5.0 of the Specific Plan).

LU 12.1.5

Require that new and recycled industrial structures and sites be designed to convey visual interest and character and to be compatible with adjacent uses, considering the:

- a. use of multiple building masses and volumes to provide visual interest and minimize the visual sense of bulk and mass;
- b. architectural design treatment of all building elevations;
- c. use of landscaping in open spaces and parking lots, including broad landscaped setbacks from principal peripheral streets;
- d. enclosure of storage areas with decorative screening or walls;
- e. location of site entries to minimize conflicts with adjacent residential neighborhoods; and
- f. mitigation of noise, odor, lighting, and other impacts.

Section 5.0 Design Guidelines and Section 6.0 Development Regulations of the Specific Plan address these issues.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce noise, odor, lighting and other impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided in Appendix C.

LU 12.1.6

Require that multiple tenant and large-scale sites developed for light industrial uses be designed with the following considerations:

- a. siting of buildings around common pedestrian walkways and public places (plazas, outdoor dining, etc.);
- b. development of pedestrian walkways, arcades, and/or other visual elements to interconnect individual buildings;
- c. use of common or compatible architectural design (materials, colors, design character, etc.);

- d. use of consistent and well-designed public and information signage; and
- e. installation of elements defining the key entry points and activity locations.

Section 5.0 Design Guidelines and Section 6.0 Development Regulations of the Specific Plan address these issues. Additionally, a Master Environmental Impact Report which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce aesthetic impacts from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C of the Specific Plan.

LU 12.1.7

Control the development of industrial uses that use, store, produce, or transport toxins, generate unacceptable levels of noise or air pollution, or result in other impacts that may adversely impact Huntington Beach.

Section 6.0 Development Regulations identifies allowable uses and assists in the control of the development of certain industrial uses that may adversely impact Huntington Beach. Additionally, a Master Environmental Impact Report which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce environmental impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C of the Specific Plan.

LU12.1.8

Require that heavy industrial uses incorporate landscape setbacks, screening walls, berms, and/or other appropriate elements that mitigate visual and operational impacts with adjacent land uses.

Sections 4.4 Landscape Plan, 5.0 Design Guidelines and 6.0 Development Regulations of the Specific Plan address these issues. Additionally, a Master Environmental Impact Report which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce visual and operational impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C of the Specific Plan.

2. URBAN DESIGN ELEMENT

The Urban Design Element focuses on the quality of the City's physical and visual character, which is determined by the organization, scale, density and pattern of the community's built environment and open spaces.

The primary goal of the Urban Design Element is to establish and strengthen community identity. An applicable goal includes:

- Enhance the visual image of the City of Huntington Beach

The following applicable Urban Design Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Nodes

Policies

UD 1.2.1

Require public improvements to enhance the existing setting for all key nodes and pedestrian areas through the consideration of the following:

- a. provide pedestrian connections and visual continuity between the node and surrounding neighborhoods;
- b. incorporate shade trees to shelter pedestrians;
- c. incorporate the use of enhanced paving materials at the pedestrian crosswalks;
- d. widen the sidewalks at intersections where feasible to minimize the length of pedestrian crossings; and
- e. enhance the connections where feasible between the public sidewalk and private commercial interior open spaces/courtyards as described in the Land Use Element by using:
 - decorative paving materials;
 - landscape materials; and
 - street furniture
- f. incorporate landscaping to mask oil operations and major utilities, such as the Edison generating station.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Sections 4.4 and 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

UD 1.2.2

Require that the nodes incorporate the public improvements specified in UD 1.2.1 and other elements that may be listed in the table, as feasible.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 4.4 and 5.3 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

Corridor Identity

Policies

UD 1.3.1

Require a consistent design theme and/or landscape design character along the community's corridors, reflecting the unique qualities of each district. Ensure that streetscape standards for the major commercial corridors, the residential corridors, and primary and secondary image corridors provide each corridor with its own identity while promoting visual continuity throughout the City.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 4.0 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

3. HOUSING ELEMENT

The Housing Element, adopted in July 1990, is intended to direct residential development and preservation in a way that coincides with the overall economic and social values of the community. The Housing Element is an official municipal response to a growing awareness of the need to provide housing for all economic segments of the community, as well as legal requirements that housing policy be made a part of the planning process. As such, the Element establishes policies that will guide City officials in daily decision making and sets forth an action program designed to enable the City to realize its housing goals. The City of Huntington Beach has adopted three goals for its housing program which are consistent with State and Regional housing policies. These goals are:

- The attainment of decent housing within a satisfying living environment for households of all socioeconomic,

racial and ethnic groups in Huntington Beach.

- The provision of a variety of housing opportunities by type, tenure, and cost for households of all sizes throughout the City.
- The development of a balanced residential environment with access to employment opportunities, community facilities, and adequate services.

These goals relate to issues which are not directly applicable to the McDonnell Centre Business Park Specific Plan, since the Specific Plan is not designated for residential uses by the City General Plan. The Housing Element objectives and policies are also not applicable.

4. HISTORIC AND CULTURAL RESOURCES ELEMENT

The overall intent of the City of Huntington Beach Historical and Cultural Element is to identify the historical resources of the community, their current designations and community status, and the issues affecting their future. Goals include:

- To promote the preservation and restoration of the sites, structures and districts which have architectural, historical, and/or archaeological significance to the City of Huntington Beach.
- Develop avenues for communication and participation in arts and cultural activities and programming to bring together diverse segments of the community.
- Highlight the City's unique cultural heritage and enhance its visual appeal.
- Expand opportunities for the City's children to receive quality experiences of arts and culture.

- Establish a wide range of arts and cultural programs and facilities that address the needs and interest of residents, workers, and visitors.

These goals relate to issues which are not directly applicable to the McDonnell Centre Business Park Specific Plan, since the Specific Plan area does not contain any significant historic and cultural resources. The Historic and Cultural Resources Element objectives and policies are also not applicable.

5. ECONOMIC DEVELOPMENT ELEMENT

The Economic Development Element is specifically concerned with the identification of a strategy to address development potentials that will broaden and stabilize the City's economic base. Its goals and policies are formulated to provide new policy direction for the City and the planning area.

The primary goal of the Economic Development Element is to provide for the economic opportunities of City's residents; business retention and expansion; and land use plan implementation. Applicable goals include:

- Provide economic opportunities for present and future Huntington Beach residents and businesses through employment and local fiscal stability.
- Aggressively retain and enhance the existing commercial, industrial and visitor serving uses while attracting new uses to Huntington Beach.
- Enhance Huntington Beach's economic development potential through strategic land use planning and sound urban design practices.

The following applicable Economic Development Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Economic Growth

Policies

ED 1.1.1

Maintain and expand economic and business development programs that encourage and stimulate business opportunities within the City.

The Specific Plan will stimulate business opportunities within the City by allowing for and encouraging development consistent with the Specific Plan under an expedited entitlement process.

Additionally, the Specific Plan provides for a range of employment opportunities in the professional, retail, service and industrial fields; thus stimulating business opportunities and widening the employee base of the community. Refer to Section 6.0 Development Standards for a list of permitted uses.

Industrial Use

Policies

ED 2.5.1

Encourage and assist existing and potential industrial owners to update, modernize, and expand their industrial properties.

The Specific Plan will encourage future industrial development by promoting a comprehensive planned Industrial Park and allowing for future "fast-track" entitlement. This Specific Plan will promote development in accordance with the Economic

Development Element. Additionally, the landowner is promoting development through its internal marketing strategies and real estate brokers.

ED 2.5.2

Seek to capture “new growth” industries such as, but not limited to:

- “knowledge” based industries, such as research and development firms (higher technology communications and information industries); communication industry service providers and equipment manufactures which are creating the next series of consumer and utility company equipment and services; biotechnical industries; environmental technology; and point of sale industries.

The McDonnell Centre Business Park Specific Plan seeks to capture “new growth” industries by allowing for and encouraging development consistent with the Specific Plan under an expedited entitlement process.

Additionally, the Specific Plan provides for a range of employment opportunities in the professional, retail, service and industrial fields; thus stimulating business opportunities and widening the employee base of the community. Refer to Section 6.0 Development Regulations for a list of permitted uses.

6. GROWTH MANAGEMENT ELEMENT

The Growth Management Element, adopted in April 1992, is a pre-requisite to establish and continue eligibility to receive monies generated by the sales tax which was approved by Orange County voters in November 1990 as Measure M (Revised Traffic Improvement and Growth Management Ordinance). The purpose and intent of the Growth Management

Element is to establish goals, policies and programs that will promote growth and development based upon the City’s ability to provide an adequate circulation system and public facilities and services.

The applicable goals of the Growth Management Element are to:

- Reduce traffic congestion
- Ensure that adequate transportation and public facilities and public services are provided for existing and future residents of the City.

The following applicable Growth Management Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Police

Policies

5.1.1

Establish a 5 minute response time for priority 1 calls (where there is a threat to life or property) for service 85% of the time.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.1.2

Continue long range planning for police services to include appropriate equipment, facilities, and staffing.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.1.3

Establish response times for priority 2 (where a threat to life or property is possible) and priority 3 (where there is no threat) calls with concurrence of Chief of Police and City Council.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.1.4

Ensure new developments meet security requirements.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes miti-

gation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Fire/Paramedic Services

5.2.1

Provide a 5 minute response time for emergency fire services 80% of the time.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.2.2

Provide a 5 minute response time for paramedic services 80% of the time.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.2.3

Ensure new development includes fire prevention methods and standards.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.2.4

Provide appropriate fire facilities, apparatus, equipment, and staffing.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public services to serve the proposed project as specified in sections 4.3. A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public services impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Traffic/Circulation

5.3.1

Increase the City's involvement with contiguous and regional agencies in circulation planning.

A Circulation Plan (refer to Section 4.2 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.3.3

Establish Level of Service "C" as the minimum acceptable standard on the City's arterial streets.

A Circulation Plan (refer to Section 4.2 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.3.4

Establish Level of Service "D" as the minimum acceptable standard on arterial intersections except those intersections included on the Deficient Intersection List established by Public Works.

A Circulation Plan (refer to Section 4.2 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.3.5

Establish a plan to fund transportation improvements.

As indicated in the Specific Plan (Section 2.5), a Growth Management Plan (GMP) is required to implement the passage of Orange County Measure M approved in the 1990 election to ensure that the planning, management and implementation of traffic improvements and public facilities are adequate to meet current and projected needs. New developments in the McDonnell Centre Business Park Specific Plan Development will be correlated with infrastructure needs. Section 4.0 of the Specific Plan presents the realistic development proposed for the planning areas and the circulation plans which will support the Land Use Plan and reinforce the design concept. In addition, a development phasing plan has been prepared as Section 4.5 of the Specific Plan. The phasing plan identifies the amount and timing of infrastructure required to support various phases of development.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.3.6

Promote traffic reduction strategies including alternate travel modes, alternate work hours, and decrease in number of vehicle trips.

Section 4.2 of the Specific Plan indicates that alternative forms of transportation should also receive careful consideration. Each future business developed within the Specific Plan will be required to address this issue.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.3.7

Participate in Measure M sales tax revenue allocation program for mitigation or construction of improvements to the existing circulation.

As indicated in the Specific Plan (Section 2.5), a Growth Management Plan (GMP) is required to implement the passage of Orange County Measure M approved in the 1990 election to ensure that the planning, management and implementation of traffic improvements and public facilities are adequate to meet current and projected needs. New developments in the McDonnell Centre Business Park Specific Plan Development will be correlated with infrastructure needs. Section 4.0 of the Specific Plan presents the realistic development proposed for the planning areas and the circulation plans which will support the Land Use Plan and reinforce the design concept. In addition, a development phasing plan has been prepared as Section 4.5 of the Specific Plan. The phasing plan identifies the amount and timing of infrastructure required to support various phases of development.

A Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce circulation impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Water

Policies

5.5.1

Provide water service to all areas with the following standards:

- a. Water pressure be provided under average and peak hour demand conditions (minimum 50 psi, maximum 80 psi, average 60-65 psi no more than 5 psi fluctuation in a 24 hour period).
- b. Provide fire flow capabilities that meet the Fire Department's requirements.
- c. Provide emergency water supply for a minimum of five days.
- d. Provide the best quality of water available at the most reasonable cost.
- e. Meet all requests for service in a timely manner.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts to water service resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.5.2

Explore the economic and practical feasibility of using reclaimed water.

The City does not currently have a reclaimed water supply system available. As indicated in Section 4.4 of the Specific Plan, reclaimed water shall be utilized where and whenever feasible and shall comply with the City's "Water Efficient Landscape Requirements" (Ordinance #1452). Usage will be addressed with future individual requests for development if such a system is available at that time.

Sewer

Policies

5.6.1

Encourage completion of the Orange County Sanitation District's Coast Trunk Sewer Line and Slater Pump Station.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts to sewer service resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.6.3

Encourage new development to comply with Orange County Sanitation District 3 and 11 requirements.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts to sewer service resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Drainage

Policies

5.7.1

Provide the Local Storm Drain System to comply with 25 year storm standard.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts to storm drain system resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

5.7.2

Encourage completion of the Santa Ana River Project and work with appropriate State, Federal, and local agencies to provide 100-year flood protection of the Santa Ana River.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce flooding

impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Land Use

Policies

5.8.1

Promote balanced growth of residential and non-residential land uses and supporting public facilities and services.

The Specific Plan prepared for this project area promotes the development of a comprehensive planned Industrial Park, consistent with the City's land use designation.

7. CIRCULATION ELEMENT

The purpose of the Circulation Element is to evaluate the transportation needs of the City and present a comprehensive transportation plan to accommodate those needs. The Circulation Element focuses on the City's arterial streets and highways; public transportation modes and services; water transportation; and air transportation.

The primary goal of the Circulation Element is to provide a multi-mode transportation system that ensures the safe and efficient movement of people and goods. Applicable goals include:

- Provide a balanced transportation system that supports the policies of the General Plan and facilitates the safe and efficient movement of people and goods throughout the City while minimizing environmental impacts.
- Provide a circulation system which supports existing, approved and planned land uses throughout the City while maintaining a desired level of service on all streets

and at all intersections.

- Develop a balanced and integrated multi-modal transportation system.
- Encourage and develop a transportation demand management (TDM) system to assist in mitigating traffic impacts and in maintaining a desired level of service on the circulation system.
- Provide sufficient, well designed and convenient on and off street parking facilities throughout the City.

The following applicable Circulation Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Streets and Highways

Policies

CE 2.1.1

Maintain a city-wide level of service (LOS) not to exceed LOS "D" for intersections during the peak hours.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities to serve the proposed project. A Circulation Plan (refer to Section 4.2 of the Specific Plan) for the project has been prepared to be consistent with the City of Huntington Beach's Circulation Element. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts

related to LOS resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

CE 2.1.2

Maintain a city-wide level of service (LOS) for links not to exceed LOS "C" for daily traffic with the exception of Pacific Coast Highway south of Brookhurst Street.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities to serve the proposed project. A Circulation Plan (refer to Section 4.2 of the Specific Plan) for the project has been prepared to be consistent with the City of Huntington Beach's Circulation Element. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts related to LOS resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Public Transportation

Policies

CE 3.1.1

Encourage and support the various public transit agencies and companies, ride sharing programs, and other incentive programs, that allow residents to utilize forms of transportation other than the private automobile.

The project Circulation Plan identifies bus stop locations along the project frontage on Bolsa Avenue. As a supplement

vehicular access to the project area, potential future access may be available from the rail line on the northern boundary of the site. The Circulation Plan preserves access to this line for future options.

CE 3.2.1

Require developers to include transit facilities, such as park-and-ride sites, bus benches, shelters, pads or turn-outs in their development plans, where feasible as specified in the City's TDM Ordinance.

The City of Huntington Beach has adopted a TDM ordinance. As discussed in Section 2.5 of the Specific Plan, all developments within this Specific Plan project will comply with City requirements.

Transportation Demand Management/Transportation Systems Management

Policies

4.1.1

Encourage non-residential development to provide employee incentives for utilizing alternatives to the conventional automobile (i.e., carpools, vanpools, buses, bicycles and walking.

Section 4.2 of the Specific Plan indicates that alternative forms of transportation should also receive careful consideration. Each future business developed within the Specific Plan will be required to address this issue.

CE 4.1.5

Promote ride sharing through publicity and provision of information to the public.

Section 4.2 of the Specific Plan indicates that alternative forms of transportation should also receive careful consideration.

Each future business developed within the Specific Plan will be required to address this issue.

CE 4.1.6

Encourage that proposals for major new non-residential developments include submission of a TDM plan to the City.

The City of Huntington Beach has adopted a TDM ordinance. As discussed in Section 2.5 of the Specific Plan, all developments within this Specific Plan project will comply with City requirements.

Parking Facilities

Policies

CE 5.1.1

Maintain an adequate supply of parking that supports the present level of demand and allows for the expected increase in private transportation use.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate supply of parking that supports the present level of demand and allows for the expected increase in private transportation use to serve the proposed project. Section 6.3 Development Standards of the Specific Plan identifies specific requirements related to the provision of parking. All developments will also be required to meet the minimum on-site parking standards of the Huntington Beach Zoning and Subdivision Ordinance. The plan will be approved by the City and the landowner will be constructing parking required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts

related to parking facilities resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

CE 5.1.2

Provide safe and convenient parking that has minimal impacts of the natural environment, the community image, or quality of life.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate supply of parking that supports the present level of demand and allows for the expected increase in private transportation use to serve the proposed project. Section 6.3 Development Standards of the Specific Plan identifies specific requirements related to the provision of parking. All developments will also be required to meet the minimum on-site parking standards of the Huntington Beach Zoning and Subdivision Ordinance. The plan will be approved by the City and the landowner will be constructing parking required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts related to parking facilities resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Scenic Highways

Policies

CE 7.1.1

Require the roadways, as shown in Figure CE-12, to be improved and maintained as local scenic highways, major

urban scenic highways, minor urban scenic highways, and landscape corridors with key entry points.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 4.4 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

CE 7.1.4

Establish landscape and urban streetscape design themes for landscape corridors, minor scenic urban corridors, and major urban scenic corridors which create a different character enhancing the corridor's surrounding land uses. For example, the design theme for corridors adjacent to residential neighborhoods should be different than the design theme for industrial or commercial uses.

The Landscape Plan for the McDonnell Centre Business Park (Section 4.4 of the Specific Plan) has been prepared to establish the design character and visual qualities of the interior and perimeter of the project area. Treatments along individual property lines shall be designed to provide security and privacy while providing visual compatibility.

CE 7.1.5

Require any bridges, culverts, drainage ditches, retaining walls and other ancillary roadway elements to be compatible and architecturally consistent with surrounding development and any other design guidelines.

As described in Section 4.4 of the Specific Plan, project area walls screening and fencing along the perimeter arterials shall provide project identity, privacy and noise control. Walls along individual property lines shall be designed to provide security and privacy while providing visual compatibility.

CE 7.1.7

Continue to construct landscaped medians in existing major and primary arterial streets and continue to require the construction of landscaped medians in new developments.

Landscape standards which require implementation of the above policy are included as Section 4.4 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

CE 7.3.1

Require that new development include landscaping that is compatible with the visual character of the designated scenic highways and corridors.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards are included as Section 4.4 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

8. PUBLIC FACILITIES AND PUBLIC SERVICES ELEMENT

The Public Facilities and Public Services Element discusses public facility service provision for Huntington Beach residents and businesses. The services discussed in this element include: law enforcement, fire protection, marine safety, education, libraries, and governmental administration.

Applicable goals include:

- Protect the community from criminal activity, reduce the incidence of crime and provide other necessary services within the City.

- Ensure adequate protection from fire and medical emergencies for Huntington Beach residents and property owners.
- Promote a strong public school system which advocates quality education. Promote the maintenance and enhancement of the existing educational systems facilities, and opportunities for students and residents of the City to enhance the quality of life for existing and future residents.

The following applicable Public Facilities and Public Services Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Police

The applicable Public Facilities and Public Services Element policies related to police have been identified within the Growth Management Element discussion, followed in italic typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency. Refer to the discussion of the Growth Management Element.

Fire/Paramedic

The applicable Public Facilities and Public Services Element policies related to fire/paramedic have been identified within the Growth Management Element discussion, followed in italic typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency. Refer to the discussion of the Growth Management Element.

9. RECREATION AND COMMUNITY SERVICES ELEMENT

The Recreation and Community Services Element has been adopted to identify, maintain and enhance local parks and recreational services and facilities.

Applicable goals include:

- Enrich the quality of life for all citizens of Huntington Beach by providing constructive and creative leisure opportunities.
- Provide adequately sized and located active and passive park lands to meet the recreational needs of existing and future residents, and to preserve natural resources within the City of Huntington Beach and its sphere of influence.
- Develop park sites to provide diverse recreational and sports facilities that meet the residents' and visitors' active and passive recreational needs.
- Ensure recreation facilities are renovated and upgraded to meet the current recreational interests of adults and youth.
- Provide parks and other open space areas that are efficiently designed to maximize use while providing cost efficient maintenance and operations.

These goals relate to issues which are not directly applicable to the McDonnell Centre Business Park Specific Plan, since the Specific Plan is not designated for residential uses by the City General Plan. The Recreation and Community Services Element objectives and policies are also not applicable.

10. UTILITIES ELEMENT

The Utilities Element discusses water supply, sanitation treatment (wastewater), storm drainage, solid waste disposal, natural gas, electricity, and telecommunications.

Applicable goals include:

- Provide a water supply system which is able to meet the projected water demands; upgrade deficient systems and expand water treatment, supply, and distribution facilities; and pursue funding sources to reduce the costs of water provision in the City.
- Provide a wastewater collection and treatment system which is able to support permitted land uses; upgrade existing deficient systems; and pursue funding sources to reduce costs of wastewater service provision in the City.
- Provide a flood control system which is able to support the permitted land uses while preserving the public safety; upgrade existing deficient systems; and pursue funding sources to reduce the costs of flood control provision in the City.
- Maintain solid waste collection and disposal services in accordance with the California Integrated Waste Management Act of 1989 (AB939), and pursue funding sources to reduce the cost of the collection and disposal services in the City.
- Maintain and expand service provision to City of Huntington Beach residences and businesses.

The following applicable Utilities Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Water

The applicable Utilities Element policies related to water supply and distribution facilities have been identified within the Growth Management Element discussion, followed in italic typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency. Refer to the discussion of the Growth Management Element.

Wastewater Treatment and Facilities

The applicable Utilities Element policies related to wastewater treatment and facilities have been identified within the Growth Management Element discussion, followed in italic typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency. Refer to the discussion of the Growth Management Element.

Storm Drainage

The applicable Utilities Element policies related to storm drainage and flood control facilities have been identified within the Growth Management Element discussion, followed in italic typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency. Refer to the discussion of the Growth Management Element.

Solid Waste

Policies

U 4.1.2

Maintain adequate solid waste collection for commercial, industrial, and residential developments in accordance with state law.

Upon the City's adoption of the McDonnell Centre Business

Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications pertaining to solid waste collection, not addressed in the Specific Plan, shall be subject to the City's adopted regulations in place at the time of any individual request.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts related to solid waste collection resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

Gas Supply, Telecommunication, Electricity

Policies

5.1.1

Continue to work with serviced providers to maintain current levels of service and facilitate improved levels of service.

The McDonnell Centre Business Park Specific Plan shall ensure the provision of adequate public facilities and utilities to serve the proposed project. A Public Facilities Plan (refer to Section 4.3 of the Specific Plan) has been prepared for the project consistent with all standards and requirements of the applicable service agencies. This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses pro-

posed by the Specific Plan.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce impacts related to gas supply, telecommunication and electricity services resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

11. ENVIRONMENTAL RESOURCES/CONSERVATION ELEMENT

The Environmental Resources/Conservation Element addresses the City of Huntington Beach's environmental resources. Applicable goals include:

- Improve and enhance the overall aesthetic value and appearance of the City of Huntington Beach through the provision and maintenance of local public and private open space.
- Protect and preserve significant habitats of plant and wildlife species, including wetlands, for their intrinsic values.
- Conserve the natural environment and resources of the community for the long-term benefit and enjoyment of its residents and visitors.

The following applicable Environmental Resources and/or Conservation Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Open Space

Policies

ERC 1.1.1

Encourage the provision of open space elements within the larger-scale development projects including but not limited to public plazas, entry courts, and planned development common areas.

As indicated in Section 4.4 Landscape Plan of the Specific Plan, each project development shall provide sufficient landscaping to continue the Landscape Plan concept, a minimum of 10 percent of the net project as well as encourage the provision of open space elements.

Aesthetic Resources

Policies

ERC 4.1.7

Include commercial, residential, industrial, and natural areas in the billboard removal programs.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

ERC 4.1.8

Include commercial, residential, industrial, and natural areas in the electrical undergrounding program.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Water, Electricity, and Gas Conservation

Policies

ERC 5.2.1

Require, the use of reclaimed water in common areas and landscape treatments of all proposed developments.

The City does not currently have a reclaimed water supply system available. As indicated in Section 4.4 of the Specific Plan, reclaimed water shall be utilized where and whenever feasible and shall comply with the City's "Water Efficient Landscape Requirements" (Ordinance #1452). Usage will be addressed with future individual requests for development if such a system is available at that time.

ERC 5.2.2

Create standards for landscaping and irrigation which are in compliance with State requirements.

Landscape standards which require individual projects to submit and implement a landscape plan consistent with the overall Specific Plan standards and in compliance with State require-

ments, are included as Section 4.4 of the Specific Plan. The landscape standards will apply to all development within the Specific Plan.

12. AIR QUALITY ELEMENT

The purpose of the Air Quality Element is to address air quality factors affecting the City, and establish goals, policies and programs in order to help achieve the goals of the Air Quality Management Plan adopted by South Coast Air Quality Management District.

An applicable goal includes:

- Improve regional air quality by a) decreasing reliance on single occupancy vehicular trips, b) increasing efficiency of transit, c) shortening vehicle trips through a more efficient jobs-housing balance and a more efficient land use pattern, and d) increasing energy efficiency.

The following applicable Air Quality Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Vehicle Work Trips

Policies

1.1.5

Encourage all new commercial, industrial, and residential structures to accommodate appropriate trip reducing activities such as alternative work schedules, on-site day-care facilities, on-site automated teller machines, "mail-in" applications, or telecommuting and/or teleconferencing facilities as technology becomes available.

The City of Huntington Beach has adopted a TDM ordinance, which addresses appropriate trip reducing activities. As discussed in Section 2.5 of the Specific Plan, all developments within this Specific Plan project will comply with City requirements.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

Transit Trips

Policies

AQ 2.1.2

Require developers of employment centers with 100 or more employees and major activity centers to include transit amenities and transit access as an integrated part of their projects.

The City of Huntington Beach has adopted a TDM ordinance, which addresses appropriate trip reducing activities. As discussed in Section 2.5 of the Specific Plan, all developments within this Specific Plan project will comply with City requirements.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

AQ 2.1.4

Encourage major commercial and industrial development projects located along transit routes to include integrated transit access points in the project design.

A Circulation Plan has been prepared for the project consistent with all standards and requirements of the applicable service agencies (refer to Section 4.2 of the Specific Plan). This plan will be approved by the City and the landowner will be constructing public infrastructure as required by the City in order to support the land uses proposed by the Specific Plan.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

Carpool and Vanpool Trips

Policies

AQ 3.1.2

Require that employment centers with 100 or more employees increase the availability and the “attractiveness” of parking spaces for vans and carpools.

The City of Huntington Beach has adopted a TDM ordinance, which addresses appropriate trip reducing activities. As discussed in Section 2.5 of the Specific Plan, all developments within this Specific Plan project will comply with City requirements.

Additionally, a Master Environmental Impact Report, which ana-

lyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce public infrastructure and service improvement impacts from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

Vehicle Trip Distances

Policies

AQ 5.1.2

Continue to encourage job growth by maintaining the supply of commercial and industrial designated land in accordance with the Land Use Map.

Consistent with the General Plan and in accordance with the Land Use Map, the Specific Plan permits job-generating land uses that will assist in improving the 1992 jobs to housing ratio. Additionally, the Specific Plan area will be incrementally developed in phases over an extended period of time as outlined in Section 4.5 of the document. The project site has been divided into a number of Planning Areas, creating distinct subareas and allowing for private development to occur in a timely manner with an overall Master Plan concept. This approach will ensure that future economic development opportunities will be implemented dependent upon market conditions. Additionally, the Specific Plan allows for flexibility in the Specific Plan Development Regulations. This flexibility in development standards is intended to accommodate future market trends and tenant needs, without sacrificing the intended high-quality character of the project area.

AQ 5.1.4

Encourage day-care facilities to be located at work sites with 100 or more employees.

Day-care facilities are permitted within all planning areas of the

Specific Plan (refer to Section 6.0 Development Regulations).
Particulate Emissions

AQ 8.1.1

Continue to enforce construction site guidelines that require truck operators to minimize particulate emission.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce particulate emissions resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C.

AQ 8.1.2

Require installation of temporary construction facilities (such as wheel washers) and implementation of construction practices that minimize dirt and soil transfer onto public roadways.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall super-

sede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to minimize dirt and soil transfer onto public roadways. A listing of the proposed mitigation measures is provided as Appendix C.

AQ 10.1.1

Continue to require the utilization and installation of energy conservation features in all new construction.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

AQ 10.1.3

Encourage energy use audits, and identify conservation mea-

tures, for all existing commercial and industrial structures. *Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.*

13. COASTAL ELEMENT

The Coastal Element, amended in 1992, includes information sufficiently detailed to indicate kinds, location and intensity of land use and applicable resource protection and development policies. The Coastal Element designates different categories of land uses which will be permitted within the coastal zone and specifies the areas where each land use map, categories and additional policies together constitute the Coastal Element, which is intended to reflect local conditions and needs while meeting the Coastal Act policies and requirements.

The Coastal Element is organized around the following issue areas which have been identified as relevant to the City's coastal zone:

- Recreation and Shoreline Access
- Visitor-Serving Facilities
- Visual Resources
- Water and Marine Resources and Diking, Dredging, Fill and Shoreline Structures

- Environmentally Sensitive Habitats
- Energy
- Community Facilities
- Coastal Land Use Plan
- Next Steps in Coastal Planning

The goals and policies within the Coastal Element provide guidance and direction for development in the coastal zone.

The goals of the Coastal Element relate to issues which are not directly applicable to the McDonnell Centre Business Park Specific Plan, since the Specific Plan area is not within the coastal zone. The Coastal Element objectives and policies are also not applicable.

14. ENVIRONMENTAL HAZARDS ELEMENT

The Environmental Hazards Element addresses flooding as it pertains to geologic, seismic and soils hazards. This Environmental Hazards Element and the referenced materials together satisfy the geologic and seismic portion of the Section 65302 (g) requirement.

Applicable goals include:

- Ensure that the number of deaths and injuries, levels of property damage, levels of economic and social disruption, and interruption of vital services resulting from seismic activity and geologic hazards shall be within levels of acceptable risk.
- Ensure the safety of the City's businesses and residents from methane hazards.
- Eliminate, to the greatest degree possible, the risk from flood hazards to life, property, public investment and social order in the City of Huntington Beach.
- Ensure the safety of the City's businesses and resident

from peat hazards.

The following applicable Environmental Hazards Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Geologic/Seismic Safety

Policies

EH 1.2.1

Require appropriate engineering and building practices for all new structures to withstand groundshaking and liquefaction such as stated in the Uniform Building Code (UBC).

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

EH 1.3.5

Encourage property owners to take adequate steps to protect their property against economic risks resulting from seismic and geologic hazards.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall super-

sede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

Additionally, a Master Plan Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to minimize dirt and soil transfer onto public roadways. A listing of the proposed mitigation measures is provided as Appendix C.

Flooding

Objective

EH 4.1

Ensure that the City's flood prevention standards and practices provide satisfactory safeguards for public and private development.

The Public Facilities Section of the Specific Plan (Section 4.3) addresses this policy. Additionally, upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained

in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

15.0 NOISE ELEMENT

The purpose of the Noise Element is to identify and appraise noise problems in the community. The Noise Element recognizes the guidelines adopted by the Office of Noise Control in the State Department of Health Services and shall analyze and quantify to the extent practicable, as determined by the legislative body, current and projected noise levels for all of the following sources:

- Highways and freeways;
- Primary arterials and major local streets;
- Passenger and freight on-line railroad operations and ground rapid transit systems;
- Aviation and airport related operations;
- Other ground stationary noise sources contributing to community noise environment.
- An applicable goal includes:
- Ensure that all necessary and appropriate actions are taken to protect Huntington Beach residents, employees, visitors and noise sensitive uses from the adverse impacts created by excessive noise levels from stationary and ambient sources.

The following applicable Noise Element policies are identified below, followed in *italic* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Ambient Noise Impacts on the Community

Policies

N 1.2.2

Require new industrial and new commercial land uses or the major expansion of existing land uses to demonstrate that the new or expanded use would not be directly responsible for causing ambient noise levels to exceed an Ldn of 65 dB(A) exterior on areas containing "noise sensitive" land uses as depicted on Figure N-1.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request. The City Noise Ordinance addresses this policy. Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce noise impacts from the buildout of future land uses. A listing of the proposed mitigation measures are provided as Appendix C.

N 1.2.5

Require development that generates increased traffic and subsequent increases in the ambient noise levels adjacent to noise

sensitive land uses to provide for appropriate mitigation measures in accordance with the acceptable limits of the City noise ordinance.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request. The City Noise Ordinance addresses this policy. Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce noise impacts from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

Traffic and Mechanical Equipment Related Noise Impacts

Policies

N 1.31

Require all new non-residential development to design and configure on-site ingress and egress points diverting traffic away from nearby "noise sensitive" land uses to the greatest degree practicable.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code

(Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request. The City Noise Ordinance addresses this policy.

Additionally, a Master Environmental Impact Report, which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce noise impacts to nearby "noise sensitive" land uses from the buildout of future land uses. A listing of the proposed mitigation measure is provided as Appendix C.

N 1.3.7

Provide for the development of alternate transportation modes such as bicycle paths and pedestrian walkways to minimize the number of noise-generating automobile trips.

Section 4.2 of the Specific Plan indicates that alternative forms of transportation should also receive careful consideration. Each future business developed within the Specific Plan will be required to address this issue.

N 1.3.8

Ensure that commercial and industrial uses, as required by the Air Quality Management Plan, implement Transportation Demand Management (TDM) programs such as incentives for car pooling, van pools, and the use of public transit.

The City of Huntington Beach has adopted a TDM ordinance. As discussed in Section 2.5 of the Specific Plan. All developments within this Specific Plan project will comply with City requirements.

16. HAZARDOUS MATERIALS ELEMENT

In February 1987, the Orange County Board of Supervisors directed the preparation of a countywide hazardous waste management plan. The Orange County Hazardous Waste Management Plan, completed in January 1989 and amended in June 1991, establishes a city and county action program for managing hazardous waste through the year 2000.

The City of Huntington Beach must implement and incorporate applicable portions of the County Plan into their General Plan and Zoning Ordinance. State law requires that implementation of the County Plan occur within 180 days of the Plan being approved by the State Department of Health Services. An applicable goal includes:

- Reduce, to the greatest degree possible, the potential for harm to life, property and the environment from hazardous materials and hazardous waste.

The following applicable Hazardous Materials Element policies are identified below, following in *italics* typeface by an explanation of how the McDonnell Centre Business Park Specific Plan achieves consistency.

Policies

HM 1.1.1

Facilitate proper disposal of hazardous waste by providing means for safe disposal.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall super-

sede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications pertaining to proper disposal of hazardous waste not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

HM 1.1.4

Implement federal, state and local regulations for the handling, storage and disposal of hazardous materials.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications related to the implementation of federal, state and local regulations for the handling, storage and disposal of hazardous materials not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

HM 1.2.2

Ensure that hazardous waste transportation activities are conducted in a manner that will minimize risks to sensitive uses.

Upon the City's adoption of the McDonnell Centre Business

Park Specific Plan, the development procedures, regulations, standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

HM 1.2.3

Support land uses or developments adjacent to or within close proximity of sensitive uses, which do not utilize, store, handle, or contain hazardous materials and/or waste, and which would create an unsafe, unhealthy, or hazardous condition for adjacent uses.

Section 6.0 Development Regulations identifies allowable uses and assists in the control of the development of certain industrial uses that may adversely impact Huntington Beach. Additionally, a Master Environmental Impact Report which analyzes buildout of the McDonnell Centre Business Park Specific Plan proposes mitigation measures to reduce environmental impacts resulting from the buildout of future land uses. A listing of the proposed mitigation measures is provided as Appendix C of the Specific Plan.

HM 1.3.1

Encourage practices and technologies which will reduce the generation of hazardous wastes at their source.

Upon the City's adoption of the McDonnell Centre Business Park Specific Plan, the development procedures, regulations,

standards and specifications of the Specific Plan shall supersede the relevant provisions of the City's zoning code (Huntington Beach Zoning and Subdivision Ordinance), as they currently exist or may be amended in the future. Section 6.0 Development Regulations of the Specific Plan provides specific development regulations and standards that will be applied to the Planning Areas contained in the Land Use Plan of this Specific Plan. All development regulations and building specifications related to the reduction of hazardous wastes not addressed in the Specific Plan shall be subject to the City's adopted regulations in place at the time of any individual request.

C. ENVIRONMENTAL MITIGATION MEASURES

Aesthetics/Urban Design

1. Prior to issuance of grading permits within the Specific Plan, the project proponent for subsequent projects located within the Specific Plan area shall submit for review and approval, an Arborist report to the Director of Public Works. This report shall detail the location and quantity of mature trees which currently exist on the specific parcel. The final landscape plan shall illustrate which trees will be removed along with the quantity and location of replacement trees.
2. Prior to issuance of building permits within the Specific Plan, the applicant shall submit a landscape construction set for review and approval to the Public Works Department. The landscape plans shall be prepared by a registered landscape architect and shall incorporate the McDonnell Centre Business Park Specific Plan requirements. Plants that are attractive to rodents should be avoided. The landscape plan shall be approved by both Public Works and Community Development

Departments.
Light and Glare

1. Prior to the approval of building permits within the Specific Plan, all exterior lighting shall be consistent with the standards established by the Zoning Ordinance (unless otherwise addressed within the Specific Plan) to minimize on and off-site light and glare impacts. The lighting shall be approved by the Community Development and Public Works Departments.
2. Prior to approval of building permits for buildings constructed within Planning Area 5, proposed lighting shall be approved by the Community Development and Public Works Departments.
3. Buildings shall emphasize the minimization of glare by incorporating non-reflective building materials. Individual building site plans shall be reviewed and approved by the City Community Development Department to assure this measure is met prior to issuance of building permits within the Specific Plan.

Transportation/Circulation

1. Prior to the issuance of building permits within the Specific Plan, each applicant shall coordinate with the City of Huntington Beach in developing a truck and construction vehicle routing plan. This plan shall specify the hours in which transport activities can occur and methods to minimize construction related impacts to adjacent residences. The final plan shall be approved by the City Engineer and Community Development Director.
2. Prior to the issuance of building permits within the Specific Plan, each applicant shall coordinate with the

City of Huntington Beach Public Works Department to ensure the following is accomplished:

- a. necessary review of signal warrants
 - b. review/approval of left turn ingress
 - c. review/approval of any added driveways
 - d. parking analysis demonstrating parking supplies meet or exceed the demands
3. Prior to the issuance of building permits within the Specific Plan, the applicant shall demonstrate to the satisfaction of the City Traffic Engineer that truck access points depicted on their "Final" site plan(s), meet the City's minimum truck turning radius standards.
 4. Prior to the issuance of building permits within the Specific Plan, the applicant shall demonstrate to the satisfaction of the City Traffic Engineer that standards (including ADA) regarding pedestrian/bicycle safety along the perimeter sidewalks have been met.
 5. The City of Huntington Beach shall collect its traffic impact fee as "interim" levels of development occurs prior to the issuance of building permits. These fees will relieve the developer of traffic mitigation obligations (as detailed for Levels 1, 2, and 3 as shown in Tables M and N of the Traffic Impact Assessment) resulting from the interim levels of development. The specific Level 1-3 improvements detailed in Tables M and N shall be added to the City's CIP and implemented in a reasonable time frame.
 6. Prior to the issuance of the first building permit within the Specific Plan, the applicant (MDRC) shall complete the intersection improvements for Westminster and Rancho identified in Table M under the Level 2 - Year 2000

- (Interim without Project) condition.
7. Prior to the issuance of the first building permit within the McDonnell Centre Specific Plan, the applicant (MDRC) shall post a bond with the City of Westminster for the Specific Plan's fair-share contribution to complete the intersection improvements for Westminster and Rancho identified in Table M under the Level 3 - Year 2000 (Interim with Project) and Level 5 - Year 2015 (Buildout with Project) conditions. The bond shall not exceed \$30,000 based on today's dollars and would be adjusted based upon the Engineering News Record Construction Cost Index. It would be activated at the time when the City of Westminster completes the identified intersection improvements. This mitigation would be unnecessary if the Cities of Westminster or Huntington Beach acquire intersection improvement funding through other efforts.
 8. An updated Traffic Impact Assessment (TIA) shall be prepared at the expense of McDonnell Douglas or successor in interest as the interim trip budget is reached. This updated TIA shall be commenced when 90% of the interim trip budget is built or has approved development applications (entitled) and no further development shall be entitled or constructed (beyond that development that generates 100% of trips for the interim trip budget) until the updated TIA and required mitigations are reviewed and approved by the City. The purpose of the updated TIA is to determine whether the trips projected for the interim condition are consistent with the actual trips and the required traffic mitigation measures for the remaining buildout of the McDonnell Center Specific Plan Area (currently estimated in Levels 4 & 5 as shown in Table 4 of the TIA). This revised TIA shall not relieve the developer of any obligation to pay any traffic impact fees (should the present or any other traffic impact fee program be in place) or provide for mitigation measures for

development at the time of developments.

9. Throughout the Specific Plan project's implementation, the City shall maintain and update an annual trip budget monitoring report to determine the status of the constructed and approved development applications (entitled) development and resulting expected trips within the McDonnell Center Specific Plan area. This annual trip budget monitoring report shall be based upon building permits issued and (entitled) development within the McDonnell Center. The trip budget monitoring report shall include gross and usable square footages of the constructed and/or entitled usages, a description of the land usage, and the trip generation rates used for the land usage proposed. The trip rates used in the monitoring report shall be those rates contained in the latest Trip Generation manual published by the Institute of Transportation Engineers (currently the 5th edition and 5th edition update) or another reliable source as approved by the City Traffic Engineer.

Air Quality

1. During grading and construction, the applicant shall be responsible for compliance with the following:
 - A. During clearing, grading, earth moving or excavation, maintain equipment engines in proper tune.
 - B. After clearing, grading, earth moving or excavation:
 1. Wet the area down, sufficient enough to form a crust on the surface with repeated soakings, as necessary, to maintain the crust and prevent dust pick up by the wind.
 2. Spread soil binders; and

3. Implement street sweeping as necessary.
- C. During construction:
 1. Use water trucks or sprinkler systems to keep all areas where vehicles move damp enough to prevent dust raised when leaving the site;
 2. Wet down areas in the late morning and after work is completed for the day;
 3. Use low sulfur fuel (.05% by weight) for construction equipment.
- D. Phase and schedule construction activities to avoid high ozone days.
- E. Discontinue construction during second stage smog alerts.
2. During grading and construction, the applicant shall be responsible for compliance with the following:
 - A. Require a phased schedule for construction activities to minimize daily emissions.
 - B. Schedule activities to minimize the amount of exposed excavated soil during and after the end of work periods.
 - C. Treat unattended construction areas with water (disturbed lands which have been, or are expected to be unused for four or more consecutive days).
 - D. Require the planting of vegetative ground cover as soon as possible on construction sites and super pads if construction is not anticipated within one month.
 - E. Install vehicle wheel-washers before the roadway entrance at construction sites.
 - F. Wash off trucks leaving site.
 - G. Require all trucks hauling dirt, sand, soil or other loose substances and building materials to be covered, or to maintain a minimum freeboard of two feet between the top of the load and the top of the truck bed sides.
 - H. Use vegetative stabilization, whenever possible, to control soil erosion from storm water especially on super pads.

Require enclosures or chemical stabilization of open storage piles of sand, dirt, or other aggregate materials.
 - J. Control off-road vehicle travel by posting driving speed limits on these roads.
3. During grading and construction, the applicant shall be responsible for assuring that vehicle movement on any unpaved surface other than water trucks shall be terminated if wind speeds exceed 15 mph.
4. During grading and construction, the applicant shall be responsible for the paving of all access aprons to the project site and the maintenance of the paving.
5. Prior to issuance of grading permits within the Specific Plan, the applicant shall be responsible for assuring that construction vehicles be equipped with proper emission

- control equipment to substantially reduce emissions.
6. Prior to issuance of grading permits within the Specific Plan, the applicant shall be responsible for the incorporation of measures to reduce construction related traffic congestion into the project grading permit. Measures, subject to the approval and verification by the Planning Department, shall include:
- Provision of rideshare incentives.
 - Provision of transit incentives for construction personnel.
 - Configuration of construction parking to minimize traffic interferences.
 - Measures to minimize obstruction of through traffic lanes.
 - Use of a flagman to guide traffic when deemed necessary.
7. Prior to the issuance of future building permits within the Specific Plan, the applicant shall provide proof to the City Community Development Director that the use will not emit objectionable odors or provide an air quality analysis including a quantitative assessment of odors and meteorological conditions consistent with the ASTM, Standard Method D1391 or Standard Method E679-79. Project design measures or additional control technology shall be implemented to ensure that odor emissions comply with SCAQMD standards.
8. Prior to the issuance of certificates of occupancy within the Specific Plan, the applicant shall prepare a Transportation Demand Management Plan (TDM) for review and approval by the SCAQMD and City. At a minimum, the plan shall include the following major elements and shall be implemented in accordance with SCAQMD Rule 1501:

- Provision of a commuter transportation coordinator, with responsibilities to include coordinating and facilitating formation of carpools and vanpools, serving as a resource person for transit information, coordinating sale of transit passes, monitoring progress towards TDM goals and surveying employees, etc.
- Provision of a commuter center which would include such information as: bus and rail transit schedules/maps; telephone numbers for the designated transportation coordinator; bus route and Metrolink schedules; ridesharing promotional material; bicycle route and facility information; and location of on-site vanpool/carpool spaces.
- Carpool and vanpool program, including participation in a computerized matching system, provision of preferential parking, and provision of travel allowances/financial incentives.
- Encouragement of non-vehicle modes, such as bicycle, walk, or bus transit.
- Transit incentives and improvements, including subsidization of transit passes and dissemination of transit information and schedules.

Noise

1. Prior to issuance of future grading permits within the Specific Plan, the applicant shall submit and have approved a noise mitigation plan to the Department of Community Development that will reduce or mitigate short-term noise impacts to nearby noise sensitive receptors. The plan shall comply with the City of

Huntington Beach Noise Ordinance and shall include, but not be limited to:

- a. A criteria of acceptable noise levels based on type and length of exposure to construction noise levels;
 - b. Physical reduction measures such as temporary noise barriers that provide separation between the source and the receptor; and
 - c. Mitigation measures such as restrictions on the time of construction for activities resulting in high noise levels.
2. Prior to issuance of future grading permits within the Specific Plan, the applicant shall produce evidence acceptable to the City Engineer that:
 - a. All grading and construction vehicles and equipment, fixed or mobile, shall be equipped and maintained with effective muffler systems that use state of the art noise attenuation.
 - b. Stockpiling and/or vehicle staging areas shall be located as far as practicable from sensitive noise receptors.
 - c. All operations shall comply with the City of Huntington Beach Noise Ordinance.
 3. Commensurate with the updated TIA (refer to Mitigation Measure 8 in Section 5.4), an updated acoustical analysis shall be performed on the following two roadway segments: 1) Rancho Road near the Navy Railroad; and 2) Rancho Road between Bolsa Chica Street and

Westminster Avenue to determine if potential vehicular noise will impact nearby residential units. The study will be prepared under the supervision of an acoustical engineer and include a discussion of the need for noise attenuation measures and/or noise barriers to ensure compliance with City noise standards. This analysis shall be submitted to and approved by the Community Development Department.

Earth Conditions

Local Geology

1. Prior to issuance of grading permits within the Specific Plan, additional studies as deemed necessary by the Director of Public Works, shall be performed to determine native elevations and evaluate the extent of compressibility of the soils for structural design purposes. These studies shall be reviewed and approved by all appropriate departments at the City of Huntington Beach.

Seismicity

2. Prior to issuance of grading permits within the Specific Plan, it shall be proven to the Department of Public Works that all structures are designed in accordance with the seismic design provisions of the Uniform Building Codes or Structural Engineers Association of California to promote safety in the event of an earthquake.
3. An engineering geologist shall be engaged to submit a report indicating the ground surface acceleration from earth movement for development parcels. All structures

shall be constructed in compliance with the g-factors as indicated by the geologist's report. Calculations for footings and structural members to withstand anticipated g-factors shall be submitted to the City for review prior to the issuance of grading permits.

Liquefaction

4. Prior to issuance of grading permits within the Specific Plan, grading plans shall demonstrate that alluvial soils shall be removed in the areas that will receive fill or foundation loading down to competent materials and recompact. Additional studies may be deemed necessary by the Director of Public Works, to evaluate the extent of liquefaction of the soils for structural design purposes.

Expansive Soils

5. Prior to approval of grading permits within the Specific Plan, the applicant shall prepare a report for approval by the Director of Public Works which assesses and provides recommendations for the following:
 - a. Specific measures for adequate foundation, paving and flatwork design in areas of any remaining expansive soils.
 - b. Identify the Expansive Index onsite and specify where necessary recommendations included, but not limited to: 1) presaturation of soils prior to concrete placement; 2) raised floors; 3) post-tensioned slabs; 4) thicker slabs; 5) deeper footings; 6) the addition of soil amendments to facilitate wetting during compaction.
6. The applicant(s) shall be responsible for remedial removal of expansive soils onsite during grading and prior to construction. Should any construction occur on expansive soils, the applicant(s) shall adhere to the recommendations identified above in Mitigation Measure 5.

Drainage/Hydrology

1. Prior to the issuance of building permits within the Specific Plan, the project applicant shall implement conditions of the Public Works Department regarding storm drainage improvements which shall include, but not be limited to:
 - Construct the necessary storm drainage improvements (identified on Exhibit 39) to handle increased flows.
 - Ensure that building pads are placed at elevations suitable to withstand 100-year flood for sites adjacent to Bolsa Chica Street between Bolsa Avenue and Rancho Road.
 - Confine street flows within the street right-of-way.
2. Prior to the issuance of building permits within the Specific Plan, the project applicant shall submit and obtain approval of final drainage and erosion control plans for each project component. These final drainage plans shall demonstrate that post-development stormwater discharge levels from the project will remain at existing stormwater discharge levels. The mitigation measures contained in the plan shall be approved by the Regional Water Quality Control Board and the City of Huntington Beach prior to any construction activities. The plans shall include measures such as the following:
 - Diversion of offsite runoff away from the construction site;
 - Prompt revegetation of proposed landscaped areas;
 - Perimeter sandbagging or temporary basins to trap sediment; and
 - Regular sprinkling of exposed soils during construction phases.

3. Prior to the issuance of building permits within the Specific Plan, the project applicant shall develop a plan to implement any recommendations from the County of Orange Flood Control Division and City Public Works Departments which will reduce impacts to the Bolsa Chica Channel floodplain resulting from onsite development. For example, one such recommendation would be the removal of the wooden bridge at a future time when it is no longer utilized by the County operations and maintenance staff to access the westerly bank of the Channel. This plan shall be submitted to the City Department of Public Works for review and approval.
4. Prior to issuance of any grading permits within the Specific Plan, the applicant shall submit a "Notice of Intent" (NOI), along with the required fee to the State Water Resources Control Board to be covered under the State NPDES General Construction permit and provide the City with a copy of the written reply containing the discharger's identification number.
5. Prior to the issuance of the building permits within the Specific Plan, the applicant shall provide a plan showing conformance to the Orange County Drainage Area Management Plan and all NPDES requirements (enacted by the EPA) for review and approval by the City Engineer. The plan shall reduce the discharge of pollutants to the maximum extent practical using management practices, control techniques and systems, design and engineering methods, and such other provisions which are appropriate.

Natural Resources/Energy

1. Building design and construction shall comply with the Energy Conservation Standards set forth in Title 24 of the California Administrative Code. Prior to approval of building permits for the Specific Plan, architectural and engineering plans shall be subject to the review and approval of the Director of Public Works to ensure conformance with these standards. Energy conservation features should include:
 - Installation of thermal insulation in walls and ceilings which meet or exceed State of California, Title 24 requirements.
 - Insulation of hot water pipes and duct systems.
 - Use of natural ventilation where possible.
 - Use of natural gas for space heating and cooking.
 - Installation of ventilation devices.
 - Orientation to sunlight and use of overhangs.
 - Landscaping with deciduous trees, to provide shade in the summer months and allow sunlight through in the winter months.
2. Prior to approval of building permits within the Specific Plan, it is recommended that the applicant consult with both the Southern California Gas Company and Southern California Edison during the building design phase for further energy conservation measures.

Public Services and Utilities

Fire

1. Prior to approval of building permits within the Specific

Plan, complete building plans shall be submitted to and approved by the Fire Department. If during the Fire Department's plan check it becomes evident that fire-ground operations will become impeded, the department will impose standard fire code requirements such as automatic sprinkler systems, alarm systems, access roads, etc.

2. Prior to issuance of building permits within the Specific Plan, the applicant shall contribute funding a "fair-share" basis towards the relocation/enlargement of the Heil station, subject to the approval of the Community Development Department.
3. Prior to issuance of building permits within the Specific Plan, the applicant shall be subject to a fire facility needs assessment/review by the Fire Department to determine the actual necessity of the new fire station and whether applications should be halted until the fire facility at Graham and Production Lane is in service.

Police

4. Prior to issuance of building permits within the Specific Plan, the need for additional police officers must be fully evaluated by the City of Huntington Beach and the applicant. If it is found that additional officers are needed to serve the area, funds must be procured to fill this position.
5. The Police Department shall be consulted during preliminary stages of the project design prior to approval of building permits within the Specific Plan to review the safety features, determine their adequacy, and suggest improvements.
6. During construction and at complete buildout, the project

shall provide easy access into and within the project site for emergency vehicles and addresses shall be well marked to facilitate response by officers. Project site plans depicting these requirements shall be reviewed and approved by the Police Department.

7. Prior to issuance of building permits within the Specific Plan, the project shall be designed such that all areas of the project will be well lit, including alcoves, walkways, doorsteps, and parking facilities. Project site plans depicting these requirements shall be reviewed and approved by the Police Department.
8. Prior to issuance of building permits within the Specific Plan, an internal security system (e.g. security guards, alarms, access limits after hours) shall be incorporated, to be reviewed by the Police Department and the City Planning Department.

Schools

9. Prior to issuance of building permits within the Specific Plan, the applicant shall provide school fees to mitigate conditions of overcrowding as part of building permit application. These fees shall be based on the state fee schedule in effect at the time of building permit applications.

Community Services

10. Prior to issuance of grading permits for Planning Area 3 in the Specific Plan resulting in removal of the existing fields, the applicant shall determine if recreation facilities are needed by existing and future employees. If deemed necessary, the applicant must enter into a lease-type agreement or provision of recreation facilities for employ-

ees to replace those lost, subject to the approval of the City of Huntington Beach Community Services Department.

Library

11. The applicant shall provide development fees to mitigate conditions of increased demand as part of building permit application. These fees shall be based on the City fee schedule in effect at the time of building permit applications.

Water

12. Prior to issuance of use and occupancy permits, the following water conservation measures shall be implemented as required by state law:
 - a. Ultra-low-flush toilets
 - b. Ultra-low-flow showers and faucets
 - c. Insulation of hot water lines in water recirculating systems
 - d. Compliance with water conservation provisions of the appropriate plumbing code
13. Prior to issuance of building permits, irrigation systems which minimize water waste shall be used to the greatest extent possible. Such measures should involve such features as the following:
 - a. Raised planters and berming in conjunction with closely spaced low volume, low angle (22 ½ degree) sprinkler heads.
 - b. Drip irrigation.
 - c. Irrigation systems controlled automatically to ensure watering during early morning or evening

hours to reduce evaporation losses.

- d. The use of reclaimed water for irrigated areas and grass lands. The project applicants shall connect to the Orange County Water District's "Green Acres" system of reclaimed water should this supply of water be available. Separate irrigation services shall be installed to ease this transition.
14. Prior to issuance of use and occupancy permits, water pressure regulators to limit downstream pressure to a maximum of 60 psi shall be installed.
15. Prior to issuance of building permits within the Specific Plan, pervious paving material shall be used whenever feasible to reduce surface water runoff and aid in ground water recharge and slopes and grades shall be controlled to discourage water waste through runoff.
16. Prior to issuance of use and occupancy permits, the applicant shall provide information to prospective occupants regarding benefits of low water use landscaping and sources of additional assistance in selecting irrigation and landscaping.
17. Prior to issuance of building permits, complete landscape and irrigation plans which minimize use of lawns and utilize warm season, drought tolerant species shall be submitted to and approved by the Water Division. Mulch shall be used extensively in all landscaped areas. Mulch applied on top of soil will improve the water-holding capacity of the soil by reducing evaporation and soil compaction. Irrigation system shall be designed to use reclaimed water when available.
18. Prior to issuance of building permits within the Specific Plan, the Water Division of the City's Public Works

Department shall be consulted during design and construction for further water conservation measures to review irrigation designs and drought tolerant plant use, as well as measures that may be incorporated into the project to reduce peak hour water demand.

19. Prior to the issuance of building permits within the Specific Plan, the project applicant shall implement conditions of the Public Works Department regarding water infrastructure improvements (identified on Exhibit 40) to handle increased water flow demands.

Solid Waste Disposal

20. To reduce the proposed project's impacts on waste disposal facilities, project designs shall develop a means of reducing the amount of waste generated both during construction and when the project is in use. The waste reduction program shall be approved by the Planning Director prior to issuance of building permits within the Specific Plan. Potential ways of reducing project waste loads include implementation of recycling programs, and use of low maintenance landscaping when possible (i.e., native vegetation instead of turf).
21. Rainbow Disposal shall be contacted during the design stage of project components to ensure the most efficient and economical means for rubbish removal. The designs shall include rubbish enclosures, projected travel areas, and turnabouts where necessary.

Public Transportation

22. Prior to issuance of building permits within the Specific Plan, a bus turnout, if determined by the City Traffic Engineer to be necessary based on roadway cross sections, travel volumes or speeds, shall be provided at each bus stop located in the project area.
23. Prior to issuance of building permits within the Specific Plan, the area adjacent to this turnout shall include a paved passenger waiting area complete with a bus shelter and bench.
24. Prior to issuance of building permits within the Specific Plan, a concrete bus pad sufficient to support the weight of a bus (see OCTD's Design Guidelines for Bus Facilities) may have to be provided at the transit stop. This would be necessary assuming the material used to construct Bolsa Avenue would be insufficient to support continued transit use of the bus stop.

Sewer

25. Prior to the issuance of building permits within the Specific Plan, the project applicant shall implement conditions of the Public Works Department regarding sewer infrastructure improvements (identified on Exhibit 41) to handle increased sewer flow demands.

Storm Drains

Please refer to Section 5.8 Drainage and Hydrology of the EIR.

Natural Gas

26. Prior to issuance of building permits within the Specific Plan, The Gas Company shall be consulted with during the building design phase for further energy conservation measures.

Electricity

27. Prior to issuance of building permits within the Specific Plan, SCE shall be consulted with during the building design phase for further energy conservation measures.

Telephone

28. Prior to issuance of building permits within the Specific Plan, building plans shall be submitted to GTE enabling GTE to assess the improvements necessary to provide adequate service to the project site.

Ord. No. 3372

STATE OF CALIFORNIA)
COUNTY OF ORANGE) ss:
CITY OF HUNTINGTON BEACH)

I, CONNIE BROCKWAY, the duly elected, qualified City Clerk of the City of Huntington Beach, and ex-officio Clerk of the City Council of said City, do hereby certify that the whole number of members of the City Council of the City of Huntington Beach is seven; that the foregoing ordinance was read to said City Council at a regular meeting thereof held on the 6th of October, 1997, and was again read to said City Council at a regular meeting thereof held on the 20th of October, 1997, and was passed and adopted by the affirmative vote of at least a majority of all the members of said City Council.

AYES: Julien, Harman, Dettloff, Bauer, Sullivan, Green, Garofalo

NOES: None

ABSENT: None

ABSTAIN: None

I, Connie Brockway CITY CLERK of the City of Huntington Beach and ex-officio Clerk of the City Council, do hereby certify that a synopsis of this ordinance has been published in the Independent on _____, 19 ____

In accordance with the City Charter of said City

Connie Brockway City Clerk

Deputy City Clerk

Connie Brockway

City Clerk and ex-officio Clerk
of the City Council of the City
of Huntington Beach, California



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